



PNJ SITE

PLACE-MAKING MASTER PLAN

PENSACOLA, FLORIDA | APRIL 9th, 2014

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AECK
SARGENT



PROJECT BACKGROUND

Daily Convo, LLC, is a single purpose entity formed to acquire and redevelop the Pensacola News-Journal property consisting of a 5.85 acre parcel on Romana and Intendencia Streets between Jefferson and Tarragona Streets in Downtown Pensacola. The redevelopment, managed by The Dawson Company, will result in a new mixed-use development including retail, residential, a YMCA facility and other commercial uses.

Given the central location and importance of this redevelopment, this package seeks to establish an urban design and development framework that will set the tone for the design of an engaging urban “place”. The ultimate goal is to create a nexus between the site and the surrounding historic districts while enabling the redevelopment to have an appealing identity of its own.

This Master Plan is not designed to create a rigid set of rules or absolute descriptions. Rather, it is intended to define a clear vision for urban place-making and to provide a set of guidelines for project designers.

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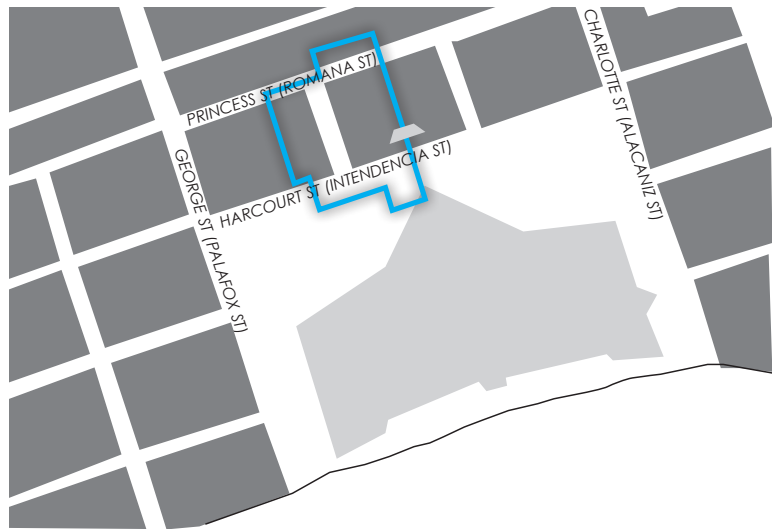


SECTION 1 | EXISTING CONTEXT

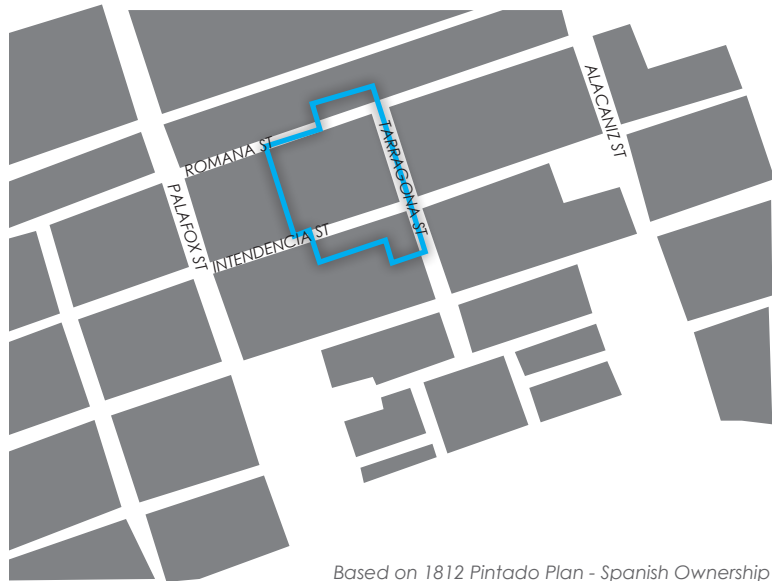


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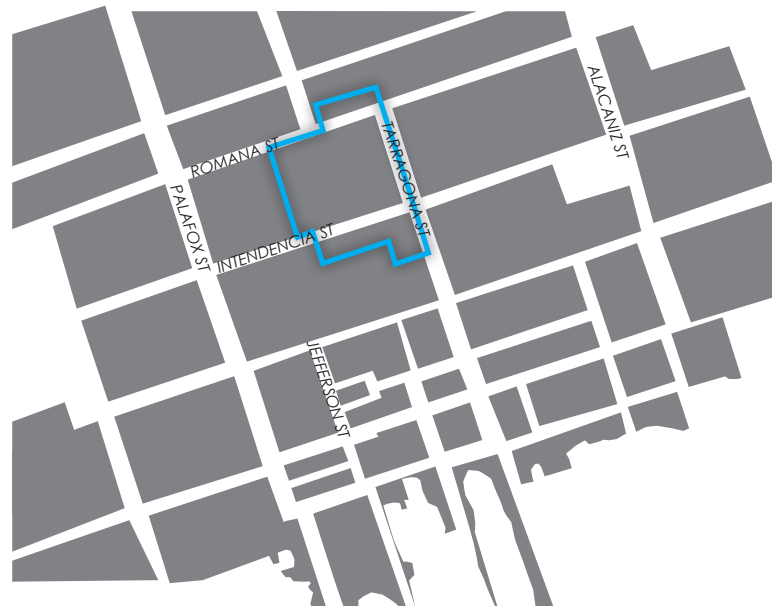
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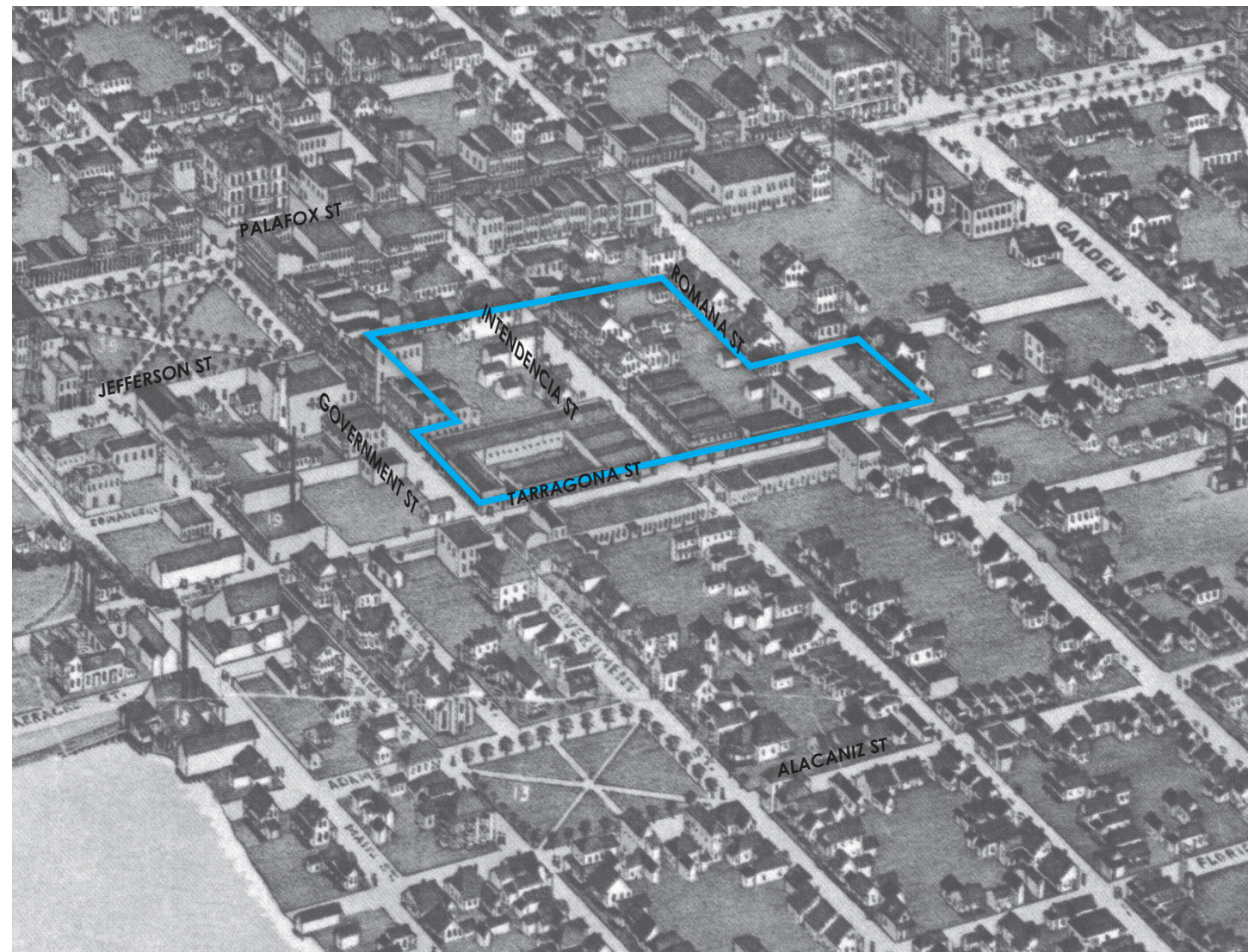
1765 - Elias Dumford Plan - British Ownership



Based on 1812 Pintado Plan - Spanish Ownership



Based on 1884 Sanborn Map - United States Ownership



1896 Koch Map - Source: Public Domain



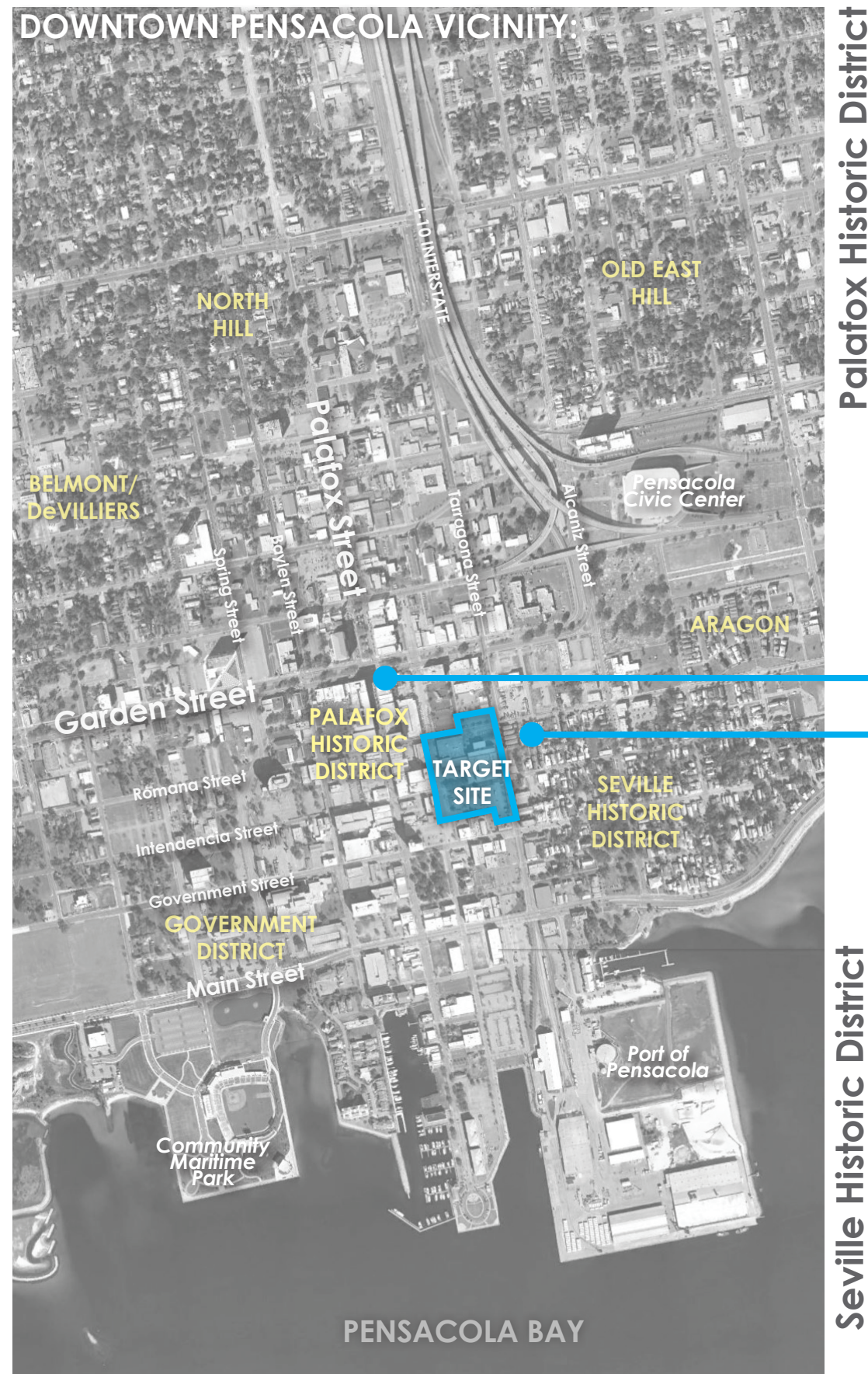
1950's Romana Street image facing west showing extension of Jefferson St and PNJ site to the south (left)
source: frankhardymademyphotographstwo.com

The development site is part of Pensacola's rich history of urban form. The city's flux in ownership has molded it in many ways but its constitutional grid has largely remained the same since the Elias Dumford Plan of 1765.

As shown in the diagrams, the site is part of the core history of Pensacola placed just north of the original British fort. Subsequently it become an integral part of the city grid during the final Spanish tenure when Tarragona was inserted as a central connection to the Spanish "Quartel" and "Calle de La Iglesia" (Church Street).

The first Sanborn map in 1884 reveals the site's historically dominant role as a commercial/ industrial asset given its proximity to the Pensacola Bay. It is here that Jefferson Street first appears in Downtown as an additional connection to the bay.

In the late 1940's Jefferson street's extension to Garden Street gave full form to the current development site. Since then the site has housed residential uses, many industrial and commercial uses, and ultimately the headquarters of the Pensacola News Journal.



Palafox Historic District



Palafox Street - Pensacola's signature Street



Pocket plaza along Palafox Street



Mid-rise structures typical at prominent corners



Contemporary take on local balcony typology

Seville Historic District



Alcaniz Street- Main corridor of Seville Neighborhood



One of many restored historic houses within the District



Contemporary multi-family building



Intendencia St - many instances of commercial adaptive re-use of single family homes

The development site lies between two of Pensacola's most notable historic districts-- the Palafox Historic District and the Seville Historic District. Both districts convey a strong and unique urban character including several defining elements that create an overall "sense of place".

Architecture & Development:

- Nearly all development in the surrounding area is built at or just slightly off the existing right-of-way.
- Commercial buildings are typically 2-3 stories with somewhat taller buildings at prominent corners
- Building entrances commonly have insets
- Palafox Street and Alcaniz Street exhibit a "Walkable Main Street" character through the use of storefront architecture
- The Seville Historic District has an emphasis on adaptive re-use of historic single-family homes
- Residential structures, both historic and contemporary, predominantly contain front porches or stoops

Open Space & Streetscapes:

- Second floor balconies are very common and typically extend within the right-of-way to create street-level arcades
- Most existing streetscapes follow an "urban" typology with an emphasis on usable sidewalks and tree wells for plantings
- Within the main commercial corridors, streetscapes contain elongated bulb-outs and expanded sidewalks to maximize outdoor activity and minimize pedestrian crossing distances
- Pocket plazas are strategically found throughout the Palafox Historic District



Pedestrian Activity: Daytime to Evening

The activity during this time of day is naturally focused within the Palafox corridor due to its diverse day time uses and its appeal as a tourist destination. While some of the restaurants and offices along Jefferson Street create some foot traffic, the activity at the development site is minimal. Though minor, the uses along Intendencia within the Palafox District and the Seville District create some east-west pedestrian activity.



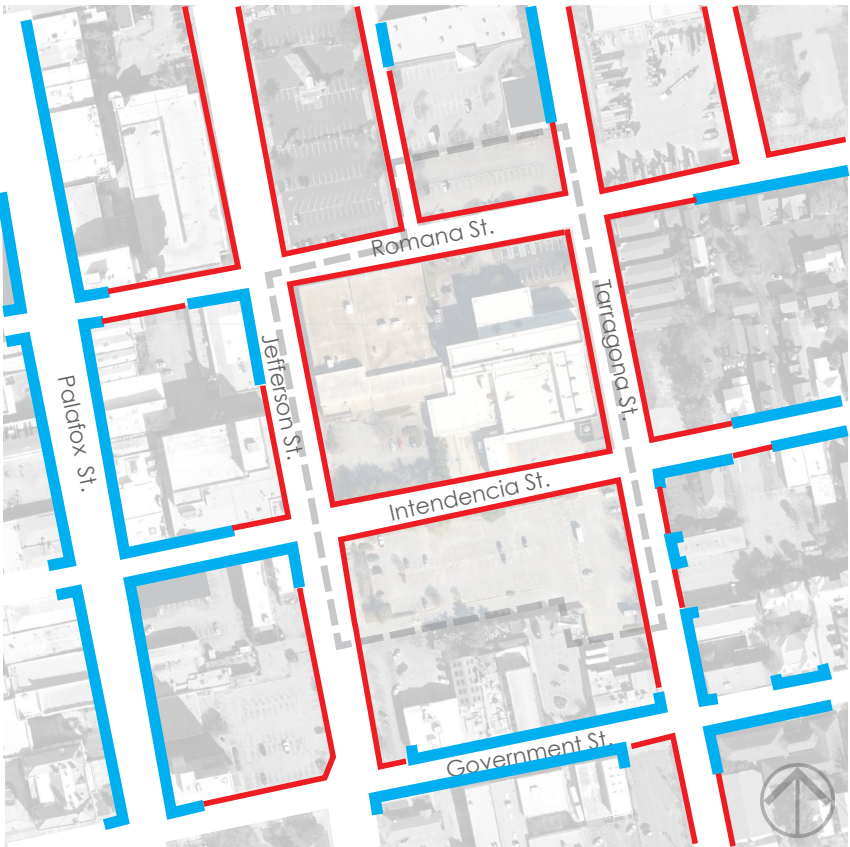
Current Land Use

- Residential
- Commercial/Retail
- Commercial/ Mixed-Use
- Office
- Bars/Night Life
- Service
- Civic
- Institutional
- Industrial
- Parking
- Vacant



Pedestrian Activity: Evening to Nighttime

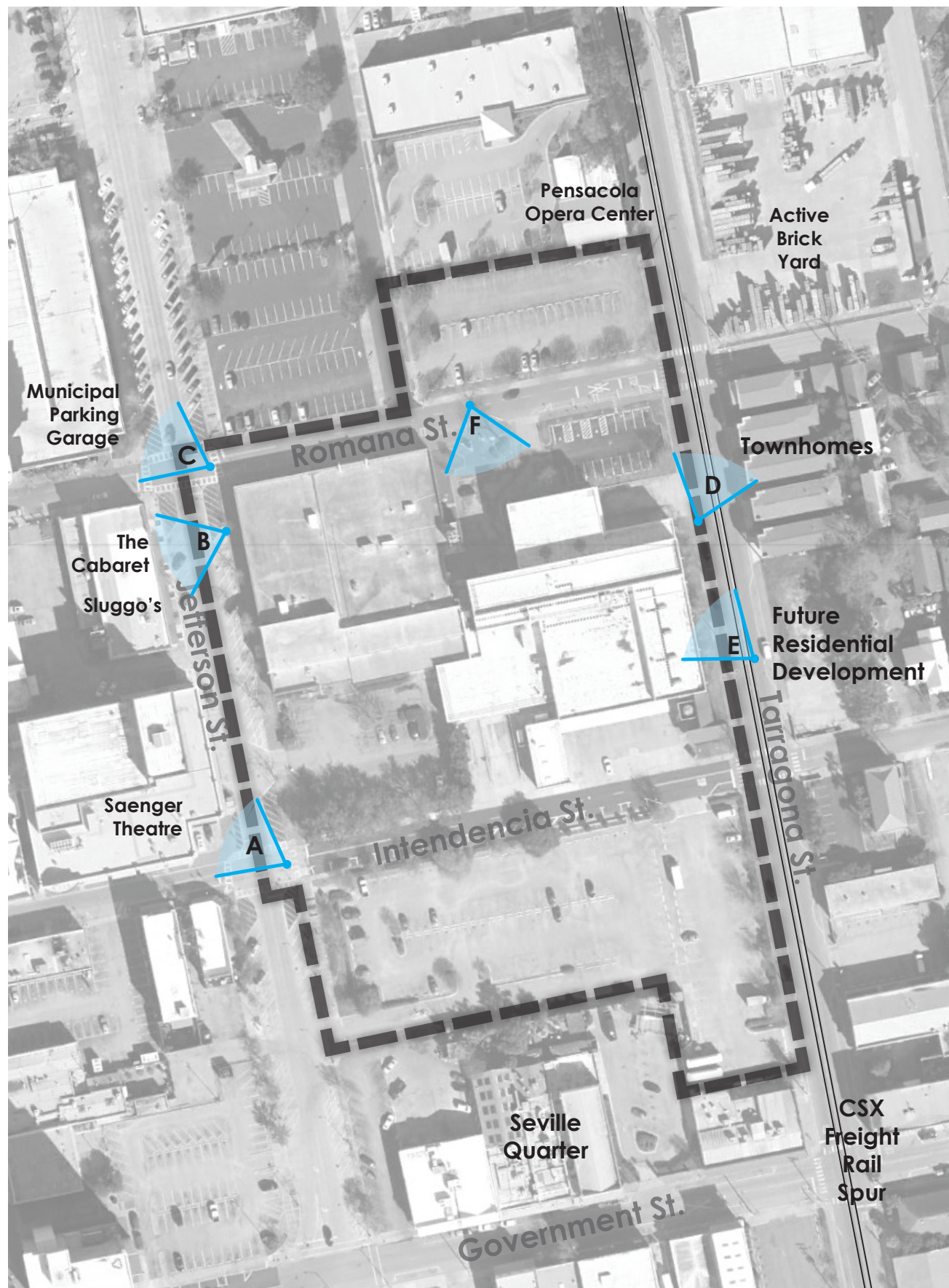
As the boutique stores and coffee shops close along Palafox Street, the activity shifts towards the bars and restaurants to the south and southwest of the development site. The nightlife scene in the area is directly adjacent to the site and is highly popular, specifically the Seville Quarter establishment. While noise is a concern given the 5 a.m. bar closing time, this activity helps adds to the “24-hour life” of Downtown.



Street Frontage Analysis

This analysis clearly reveals the gaps in “street activation” that surround the development site. It is evident that this development will be a catalyst for generating activity between the two historic districts. With the exception of this site, Intendencia Street is the only east-west street within the immediate context that is almost fully activated from Palafox Street to Alcaniz Street in the

- Activated Street Front
- Non-Activated Street Front



A - The loading area of the Saenger Theatre facing the development site is the only "back door" portion of that intersection



B - Two restaurant/bars facing Jefferson Street assist in activating this section of the block



C - Municipal parking looks dated and adds to the lack of activity along Jefferson Street but provides an additional source of parking for the project



D - Tarragona Street has an emerging residential character adjacent to the development site. However, the active rail line and brick yard may pose challenges as this street transitions



E - Overhead utilities along Romana Street and Tarragona Street affect residential marketability and are not compatible in an urban "place-making" context



F - The brick pavers along Jefferson Street and Romana Street are of historic relevance and should be salvaged or repurposed



A - Jefferson Street



C - Tarragona Street

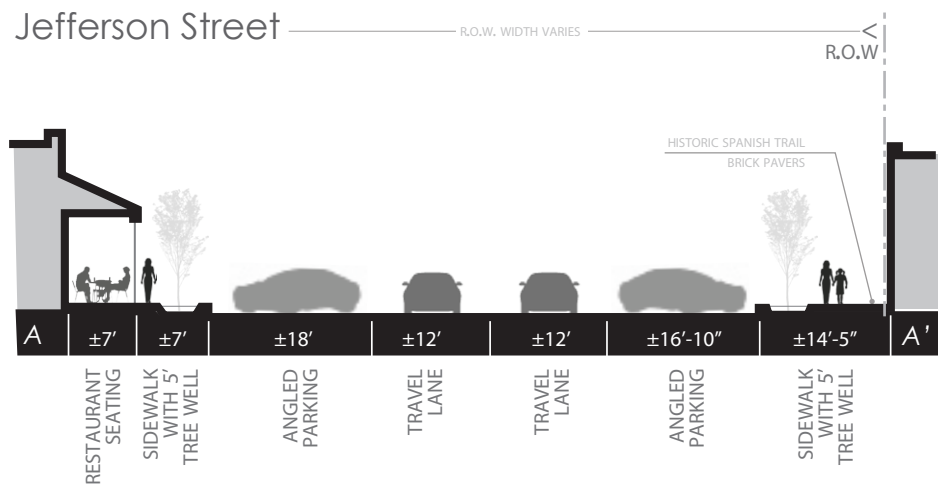


E - Intendencia Street



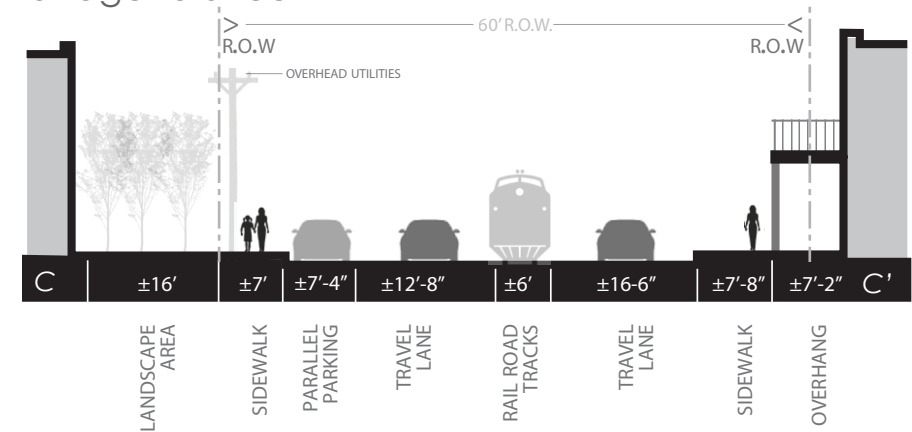
F - Romana Street

Jefferson Street



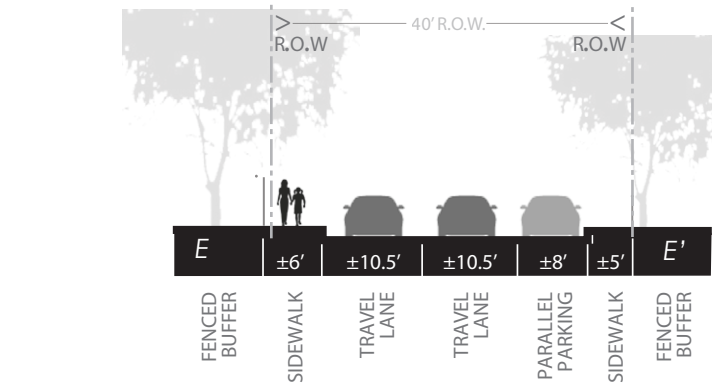
Jefferson Street currently functions as a de facto "service" street for the Palafox Corridor due to a vast amount of parking and few building "frontages". However, its current ample curb-to-curb dimension creates spatial opportunities for pedestrian improvements.

Tarragona Street



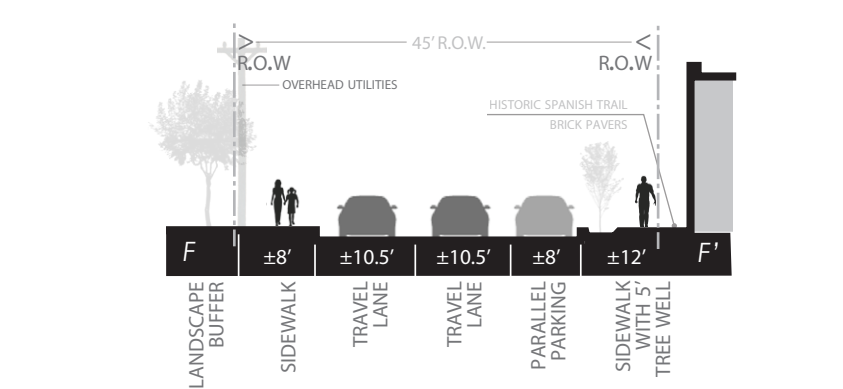
Tarragona Street is hindered by an active rail line that creates the perception of a divide between the Palafox District and the Seville District, though trains only pass a few times a day. Its limited sidewalk space within the right-of-way makes improvements difficult without reducing lane widths or providing easements in to the development property. Along with the space constraints, the existing overhead utility lines and poles need to be taken in to consideration.

Intendencia Street



Intendencia Street offers the best opportunities for place making based on the existing mature Live Oak trees and narrow street width.

Romana Street



Romana Street has plenty of space for improvements. However, the existing overhead utility lines and poles need to be taken in to consideration



SECTION 2 | URBAN DESIGN FRAMEWORK



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Vibrancy & Mix of Use



This project will ultimately live within the heart of downtown Pensacola, a vibrant historic urban core. In order to match the intensity and life of downtown and to contribute added vibrancy, this project should incorporate a wide variety of uses and features.

- Include housing, office, retail and recreation/health/wellness (e.g., YMCA) land uses
- Incorporate a diverse mix of commercial tenants including providing local services along with more regional destinations/attractions
- Be mindful of views into buildings and views from within buildings looking out in order to maximize the visibility and energy of urban life

Open Spaces



A key component in attracting new residents to live in urban setting is to provide high-quality usable open space, even in small amounts. In addition, this project will ultimately serve more than just the residents living on site and therefore has a responsibility to provide open spaces that are publicly accessible to all.

- Create public, semi-private and private open spaces as part of the development
- Design the public sidewalk as “open space” including static nodes along the active walkways

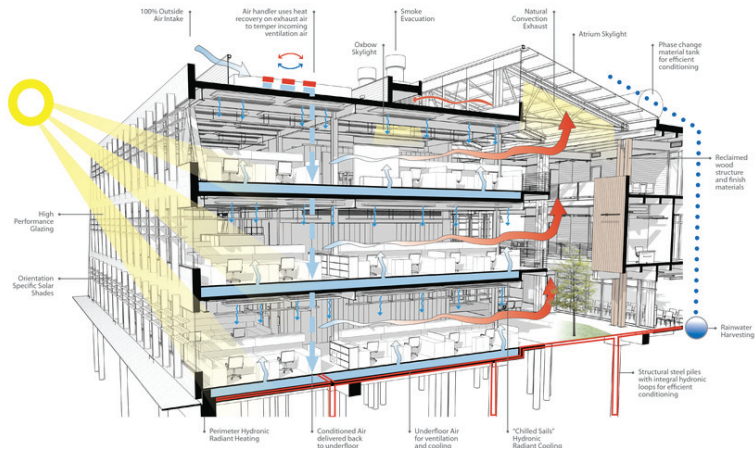
Walkability & Pedestrian Scale



This project should recognize that it sits within a broader highly walkable setting and should strive to create a high-quality pedestrian environment, both internally and externally.

- Adopt a mentality in which all street frontages are considered as “fronts” with no “back side” of the development facing a public street
- Incorporate wide sidewalks on all public streets with street furniture, street trees, street lighting and decorative elements
- Design buildings at a “human/pedestrian scale” and don't let parking dominate the design

Sustainability



Collectively, the urban design fundamentals outlined here seek to inform a design that results in a “place” rather than just a “project.” In this way, this project should be designed with long term sustainability and placemaking attributes in mind.

- Incorporate current best practices of building efficiency and sustainability including LEED principles,

Compatibility & Context



This project sits within an existing urban context including two historic districts, a thriving commercial core and a stable intown neighborhood. Therefore, this project has the responsibility to be sensitive and responsive to its surroundings.

- Avoid monolithic development that overwhelms the surrounding context
- Incorporate scale and massing features that vary significantly within block faces
- Recognize that the Seville Neighborhood to the east (“neighborhood”) works at a different scale and context than the Palafox Business District to the west (“urban core”)
- Be mindful of the fact that this site has a very long history in the life of Pensacola including pre-revolutionary fortifications. Preserve and/or reuse historic artifacts to the extent feasible.

Realism



While seeking to design a visionary development it is equally important to be mindful of economic, operational and physical realities in order to ensure that this project can actually be achieved.

- While parking should be carefully designed and located so as not to be the dominant feature, provide enough parking to meet market demands
- Design the project with the ability to phase the construction
- Be mindful of the location and appearance of service and loading
- Be mindful of construction costs and the impacts of design decisions on cost and affordability

ACCESS & CIRCULATION FRAMEWORK

PRECEDENTS



ACCESS- C: Development with well-hidden parking garage entry



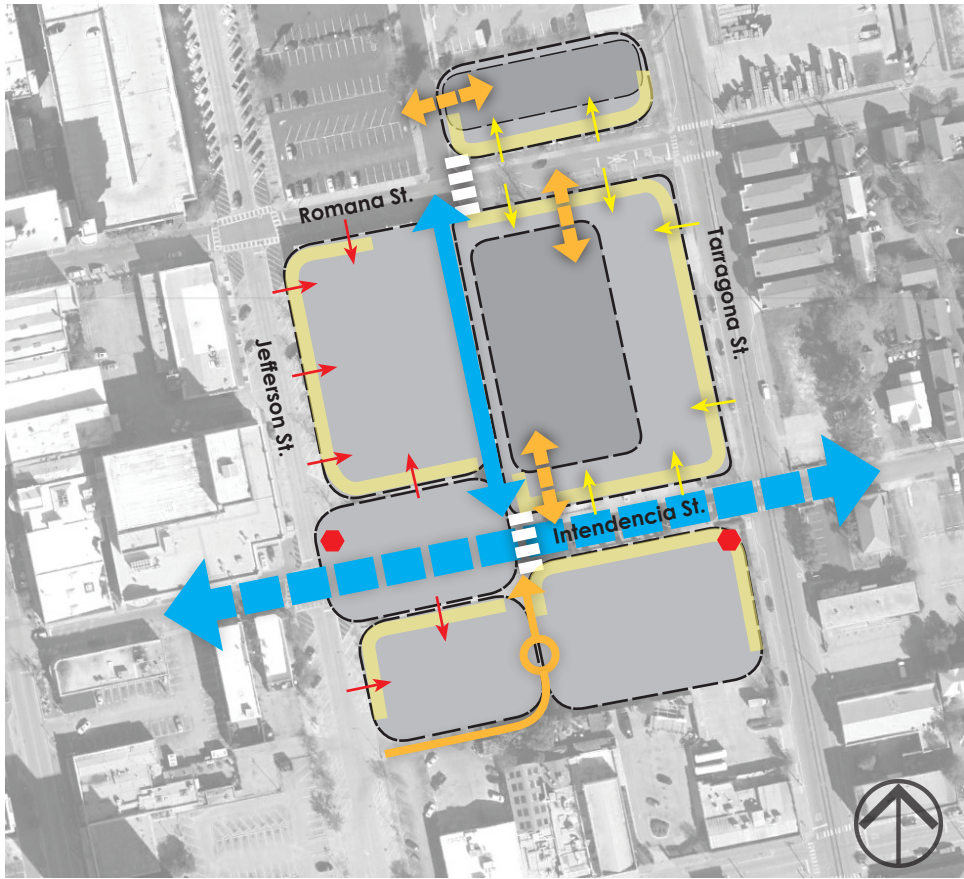
LAND USE- B: Example of Commercial Mixed-use building



LAND USE- D: Example of contemporary YMCA in urban context

- A-** Intendencia Street: Primary east-west pedestrian connection between Seville Historic District and Palafox Business District
- B.1-** New north-south pedestrian "paseo" to provide public access to shared parking deck and better overall connectivity
- B.2-** New mid-block pedestrian crossings at Intendencia and Romana
- C-** Multiple access points to shared parking deck (Intendencia and Romana)
- D-** No new curb cuts along Tarragona Street
- E-** Multiple storefront entries along Intendencia and Jefferson
- F-** Multiple building entry points to help activate the street (YMCA, commercial mixed-use building, residential buildings/lobbies)
- G-** Individual unit entries for ground floor residences to help activate the street
- H-** Drop-off for YMCA via internal alley from Jefferson St.
- I-** Gateway/Signage Element
- J-** Improved streetscapes on all public streets to enhance walkability for entire district (See Proposed Streetscapes sheet)

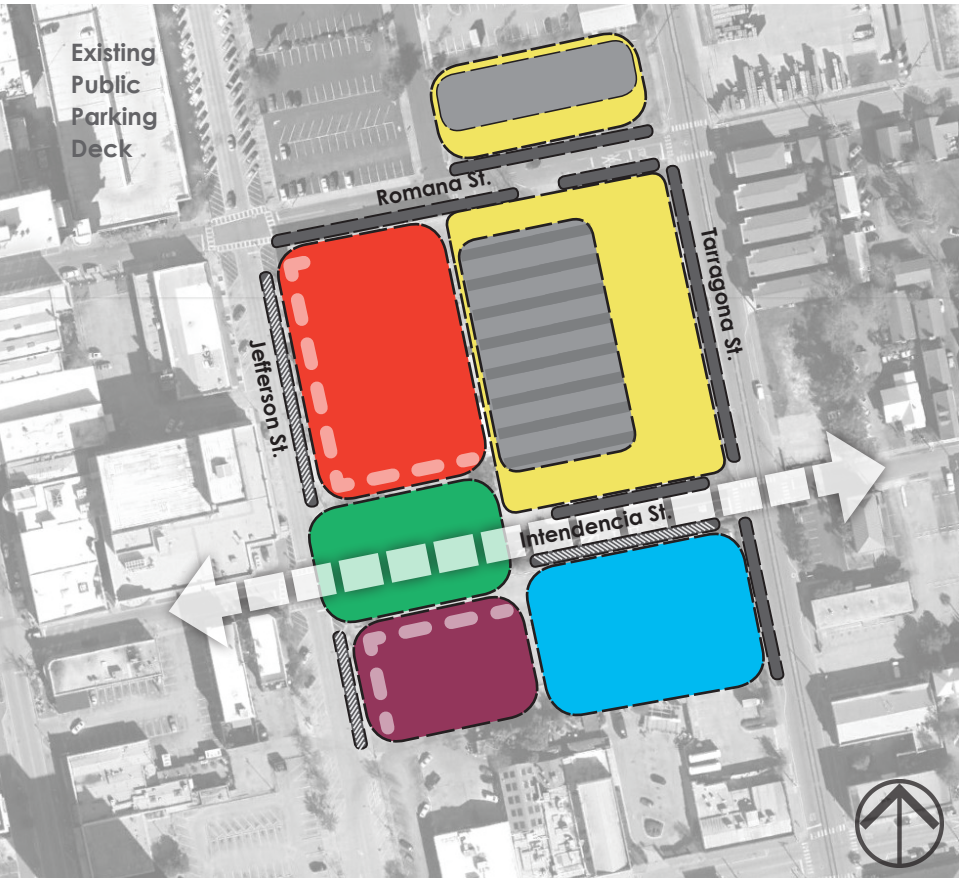
- A-** Main Pedestrian Corridor
- B.1-** Pedestrian Paseo
- B.2-** Mid-Block Crossing
- C-** Parking Deck Access
- E-** Multiple Store Front Entries
- F-** Activated Street Fronts
- G-** Multiple Ground Floor Residential Entries
- H-** YMCA Drop-off
- I-** Potential location for gateway element



LAND USE & PARKING FRAMEWORK

- A- Residential Mixed-Use:** 4 stories of multifamily apartments over storefront retail/commercial (5 stories total)
- B- Commercial Mixed-Use:** 2 stories of office over storefront retail/commercial (3 stories total)
- C- Urban Residential** 4 stories of multifamily apartments with structured parking (4 stories total)
- D- YMCA** 1-2 stories
- E- Live Oak Plaza** Signature public space – See Open Spaces Sheet
- F- Storefront Streets** Focus on neighborhood and service-oriented retail
- G- On-Street Public Parking**
 - G.1-Parallel parking (Intendencia, Romana, Tarragona)
 - G.2-Angled parking (Jefferson, Intendencia)
- H- Structured Parking - Shared** Located in block interior, screened from view Lower level(s) for district-wide public use (+/- 387 spaces) Upper levels for private residential parking; access controlled
- I- Structured Parking – Private** Podium style for private residential parking; access controlled Ground floor screened from view along public rights-of-way

- A-** Residential Mixed-Use
- B-** Commercial Mixed-Use
- C-** Urban Residential
- D-** YMCA
- E-** Live Oak Plaza
- F-** Storefront Street
- G.1-** On-street Angled Parking
- G.2-** On-Street Parallel Parking
- H-** Structured Parking- Shared
- I-** Structured Parking- Private





C- Small step backs above retail



D.2- Varied architectural expressions/materials



D.3- Appearance of multiple building bays



E- Small insets at entries



F- Extended second floor balconies



G- Upper story balconies + step backs

A- Building heights step down at the east end of the project to reflect the smaller scale of the Seville Historic District

A.1 Jefferson Street Frontages:
Mixed-Use Residential = +/-5 stories tall, Commercial Mixed-Use = +/-3 stories tall

A.2 Tarragona Street Frontages:
Residential = 3-4 stories tall, YMCA = 2-3 stories tall

B- Break up building massing along Tarragona Street to help transition into the Seville Historic District and reduce exposure of building front to locomotive noise

C- Small building step backs (5-10 feet) at the juncture between storefront retail and upper level uses to help reinforce a scale that is compatible with Palafox Place-- does not have to be continuous

D- Varied massing/scale features within each block face to avoid a monolithic appearance

D.1- Varied cornice heights

D.2- Varied architectural expression/details/materials

D.3- Appearance of multiple building bays

E- Small insets at main building entries (but not necessarily every storefront)

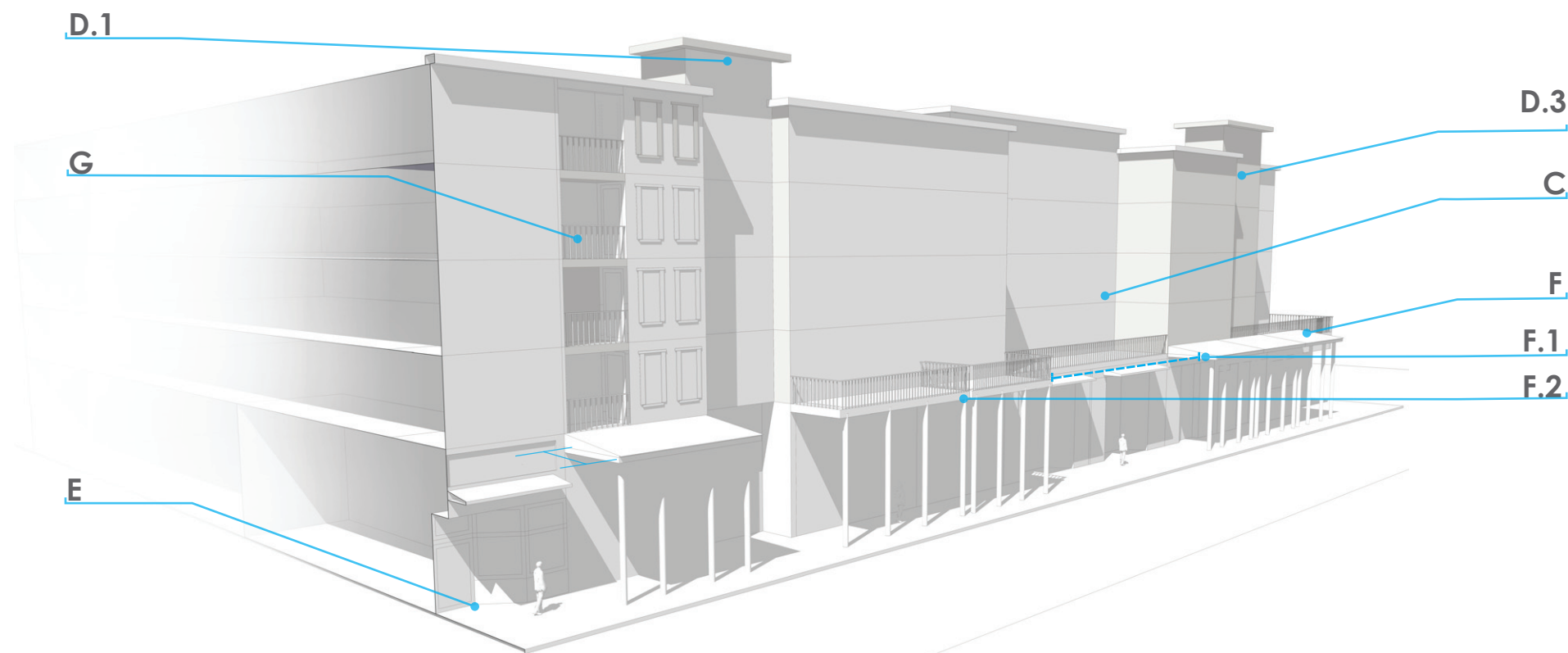
F- Use second level balconies or overhangs to create arcades over some storefront retail locations

F.1- Avoid long, uninterrupted arcades over entire block faces

F.2- Some second level balconies should extend out over public sidewalk right-of-way

G- Upper story residential balconies to create additional "eyes on the street"

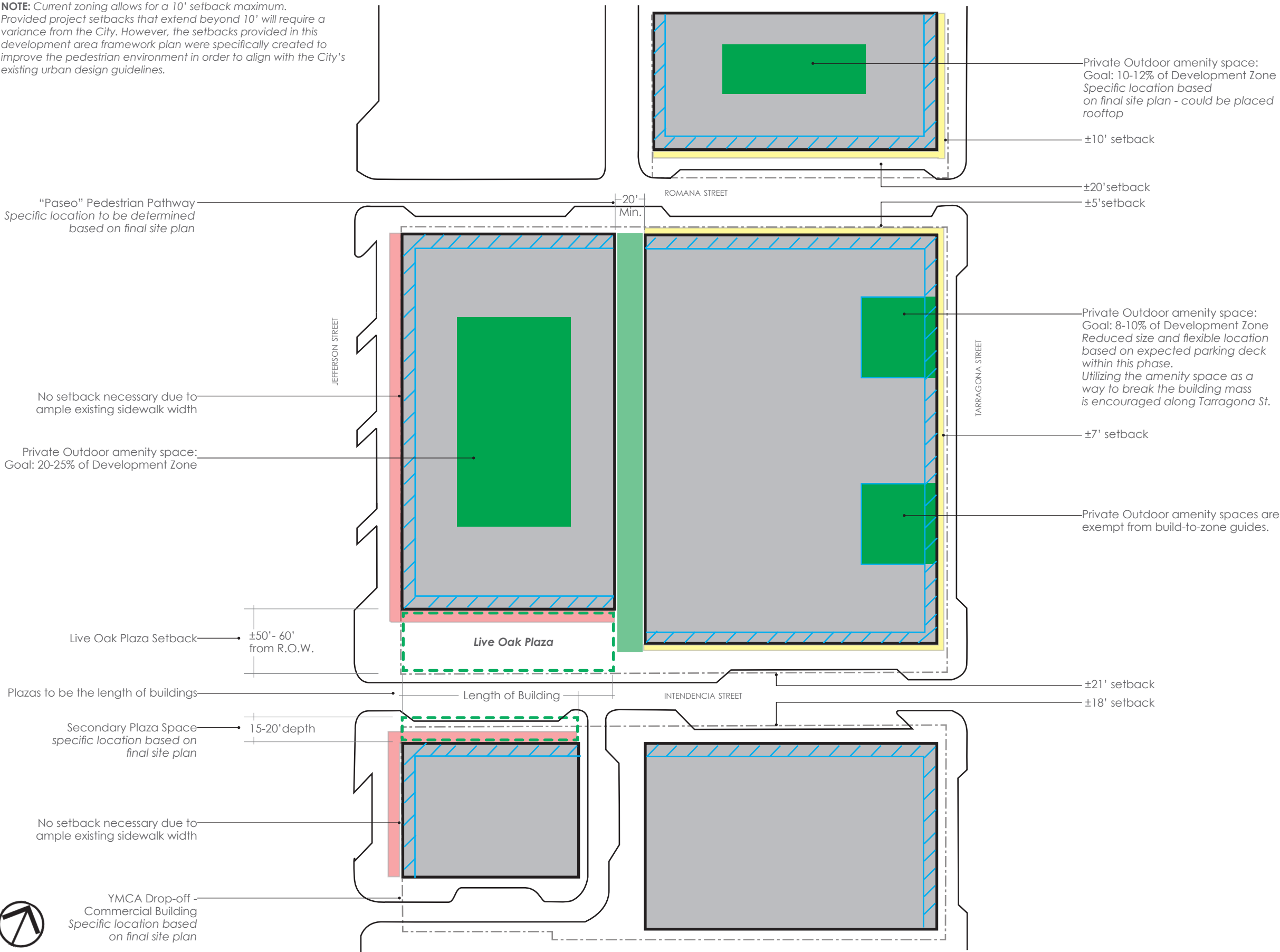
Massing & Scale Guidelines



A/B- Building Height & Massing



NOTE: Current zoning allows for a 10' setback maximum. Provided project setbacks that extend beyond 10' will require a variance from the City. However, the setbacks provided in this development area framework plan were specifically created to improve the pedestrian environment in order to align with the City's existing urban design guidelines.



The development area framework serves as a baseline diagram to help inform building footprint parameters in relation to the public right-of-way and private/public open spaces.

- Development Area**
Building footprints should not extend beyond these areas
- Balcony Overhang Zone**
8 -10' area where second floor balconies may extend into the right-of-way to create arcades
- Supplemental Zone**
5-6' - Area reserved for transitional elements between building/unit entrance and public right-of-way. Well designed stoops, porches, or similar elements should be utilized to create a sense of privacy for residents.
- Build-to-Zone**
0-10' area in which the front face of the building should be located. Building face may undulate within this zone allowing for elements such as entrance insets and forecourts. Exceptions to this zone include courtyards, entry plazas, porches, and stoops.
- Private Outdoor Amenity Space**
For each phase, outdoor amenity space may be met in one contiguous open area or multiple areas.
- Paseo Walkway**
-A minimum 20' wide publicly accessible pedestrian walkway. Design should incorporate and enhance building facades facing the Paseo
- Publicly Accessible Plaza**
-Live Oak Plaza: Provided dimensions should be maintained in order to enable preservation of Live Oaks and a proper plaza space
-Secondary Plaza Space: Location contingent upon final design of building.
- Existing Property Line**

PRECEDENTS



A - Potential for Live Oak Plaza



B - Pedestrian alley / "Paseo"



C - Entry Plazas



D -Semi-private residential stoops



E- Private residential courtyards



E.2 - Potential for Tarragona Street Courtyards

OPEN SPACE TYPES

A- Live Oak Plaza

- Preservation of existing Live Oaks on the north side of Intendencia Street
- Building edges set back +/-50'-60' on the north side of Intendencia and at a shorter depth on the south side
- Reuse of historic bricks in plaza and in the street to create a pedestrian zone

B- Paseo

- Mid-block public north-south pedestrian connection from Intendencia to Romana; provides public access through block and to/ from shared parking deck
- Reuse of historic brick pavers in Paseo to emphasize public pedestrian access

C-Entry Plazas

- Small inset entry plazas at key building entries to help create a transition from public to private

D- Semi-Private Residential Stoops

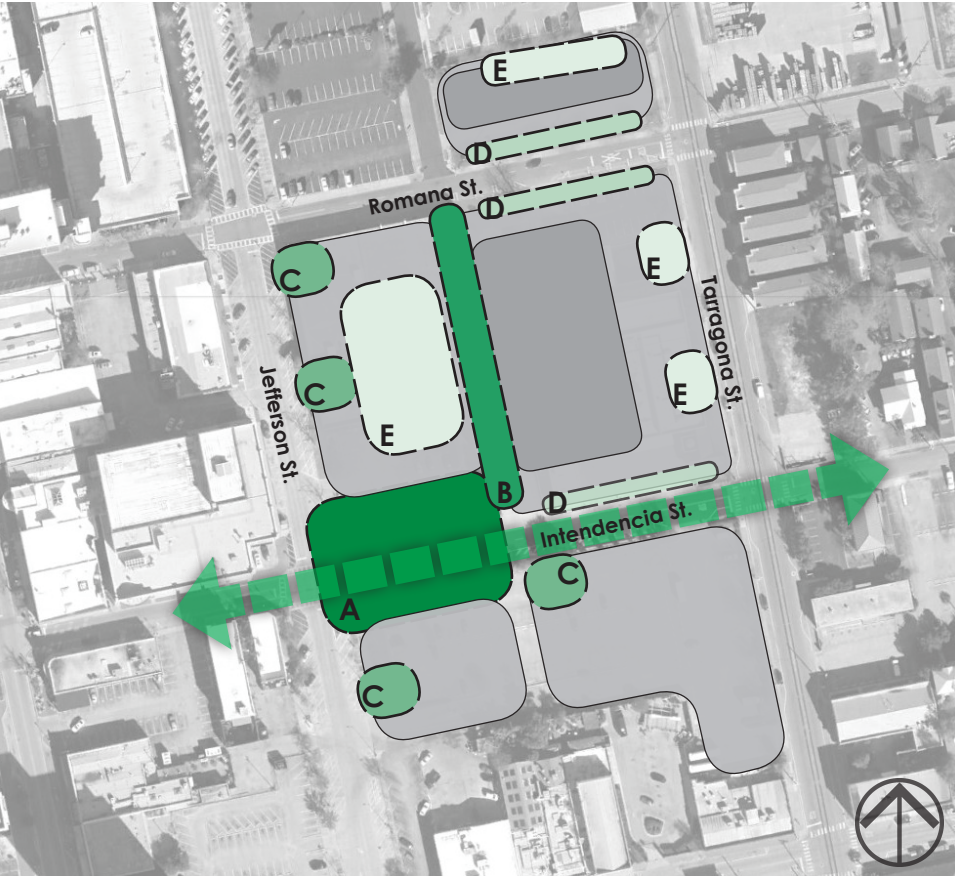
- Individual raised unit entries for ground floor residences
- Landscaping between entries/ stoops to help create small transition from public to private space

E.1- Private Amenity Courtyards

- Private courtyards – minimum one per residential building/phase

E.2-Taragona courtyard(s) help break the building scale/massing

Locations can vary

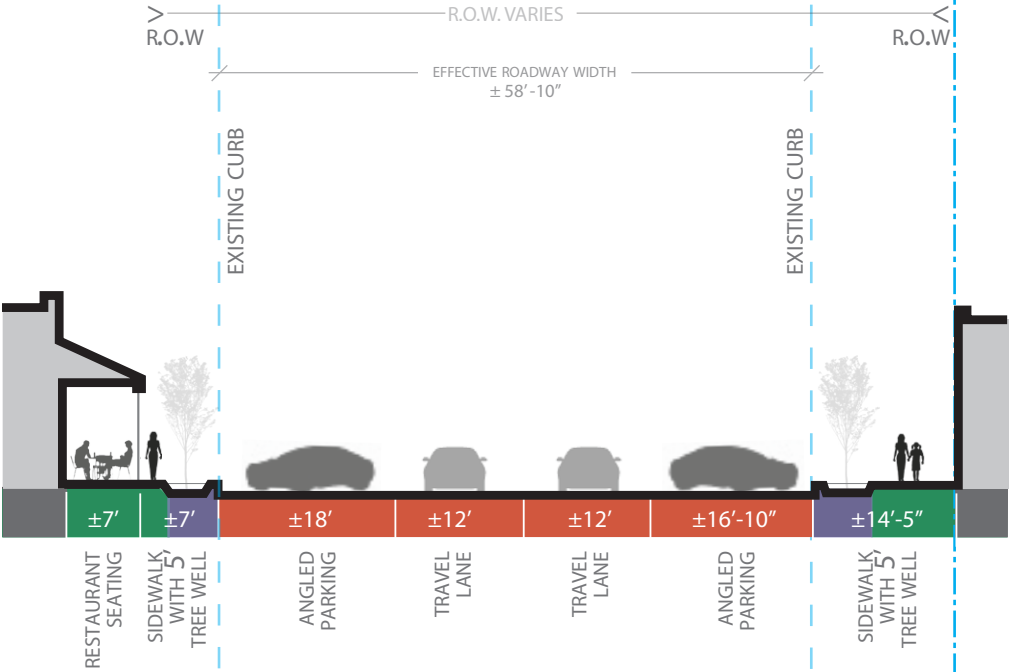


Less Public

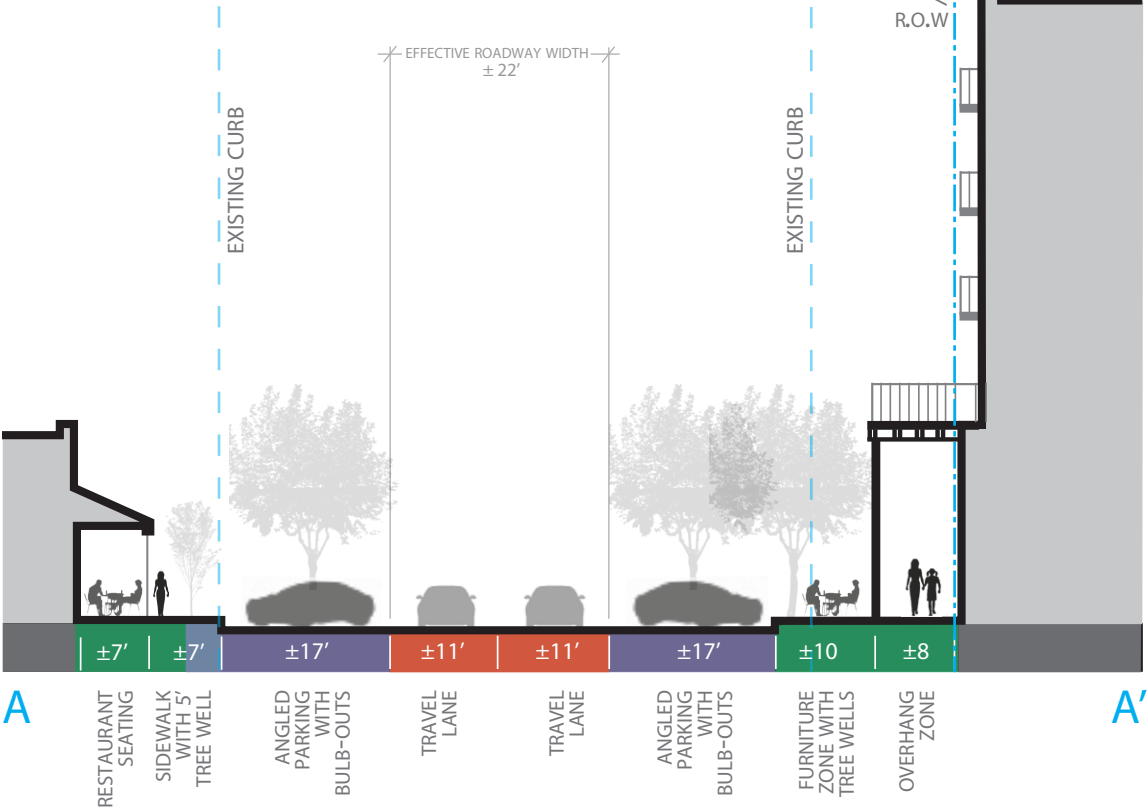
More Public

Jefferson Street

Before

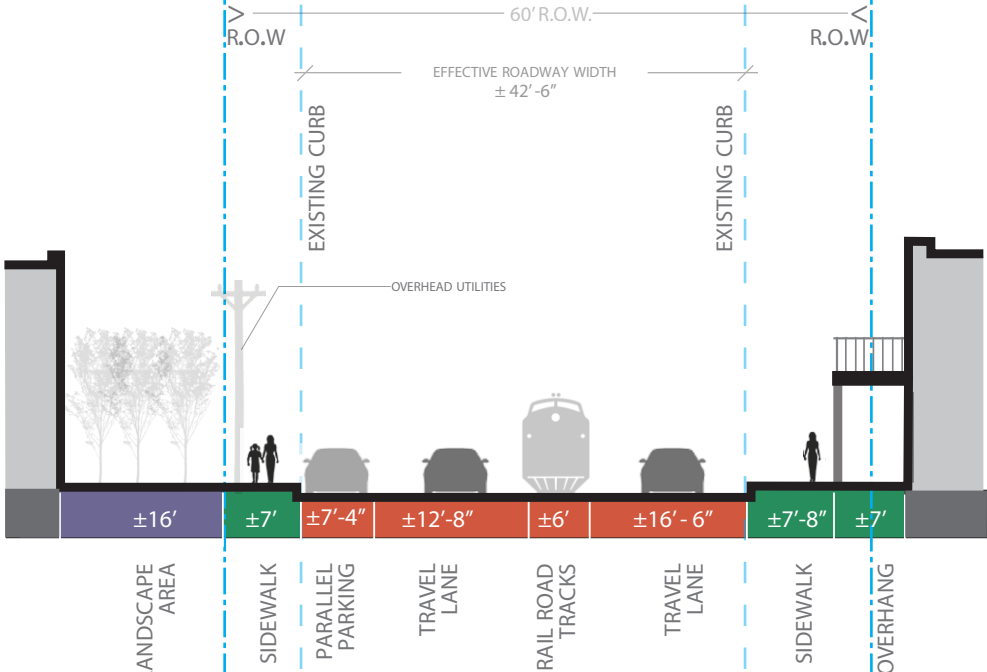


After

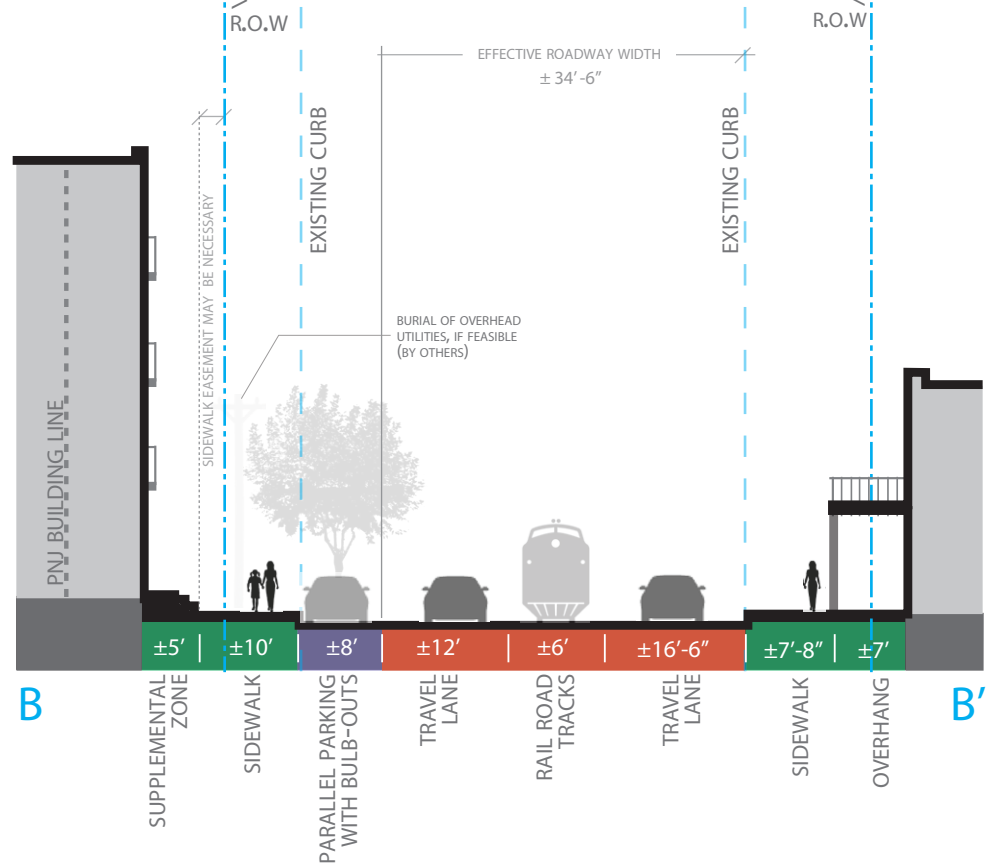


Tarragona Street

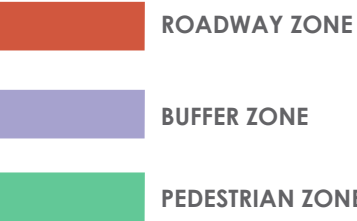
Before



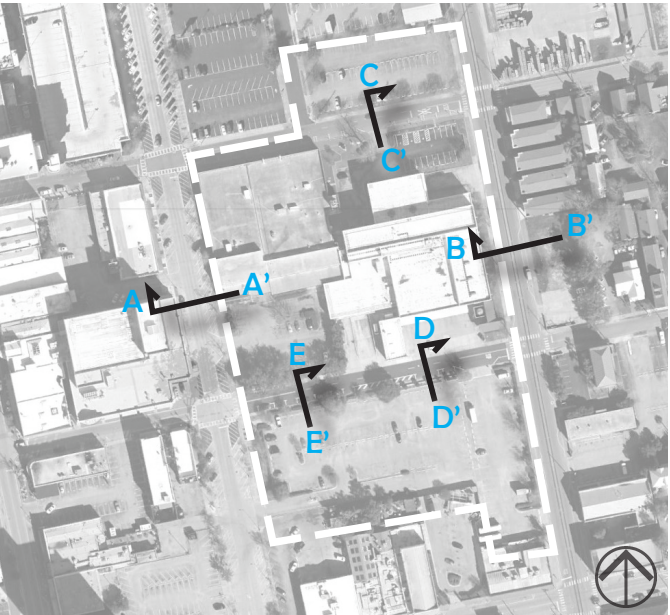
After



Street Section Zones



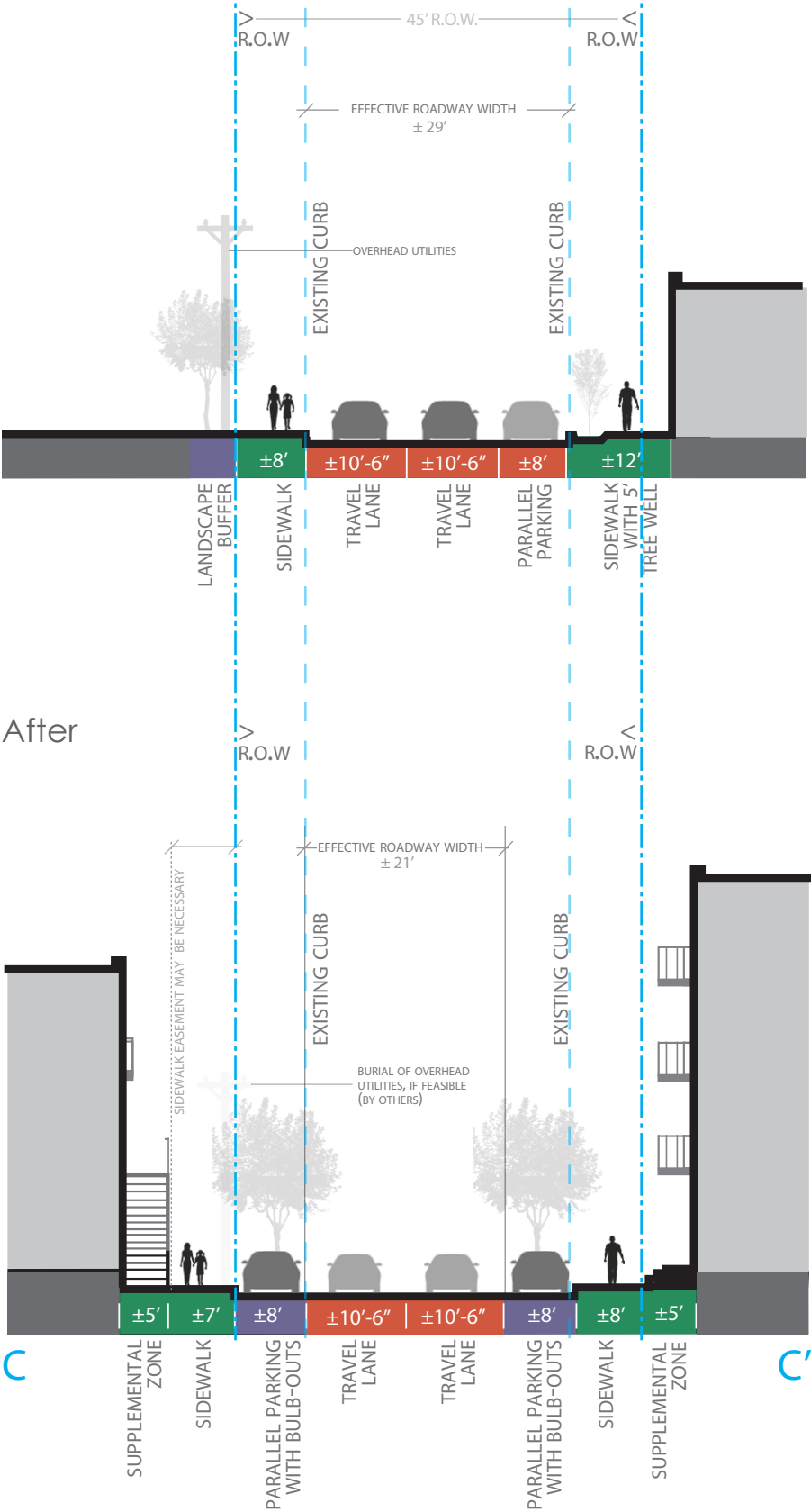
- A- Jefferson Street**
 - Narrow street width to create a more "urbanized" street
 - Formalize bulb-outs at intersections; shorten pedestrian crossing widths
 - Insert landscape/tree islands to give the street a more human scale
- B- Tarragona Street**
 - Formalize on-street parallel parking to help create buffer for pedestrians
 - Formalize bulb-outs at intersections; shorten pedestrian crossing widths
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- C- Romana Street**
 - Increase on-street parallel parking for residential blocks
 - If feasible, bury utilities
- D/E- Intendencia Street**
 - Primary east-west pedestrian connection between Seville and Palafox
 - Increase on-street parking – angled for YMCA, parallel for residential
 - Increased setbacks and landscaping for Live Oak Plaza



See Suggested Landscape Masterplan for Plan View

Romana Street

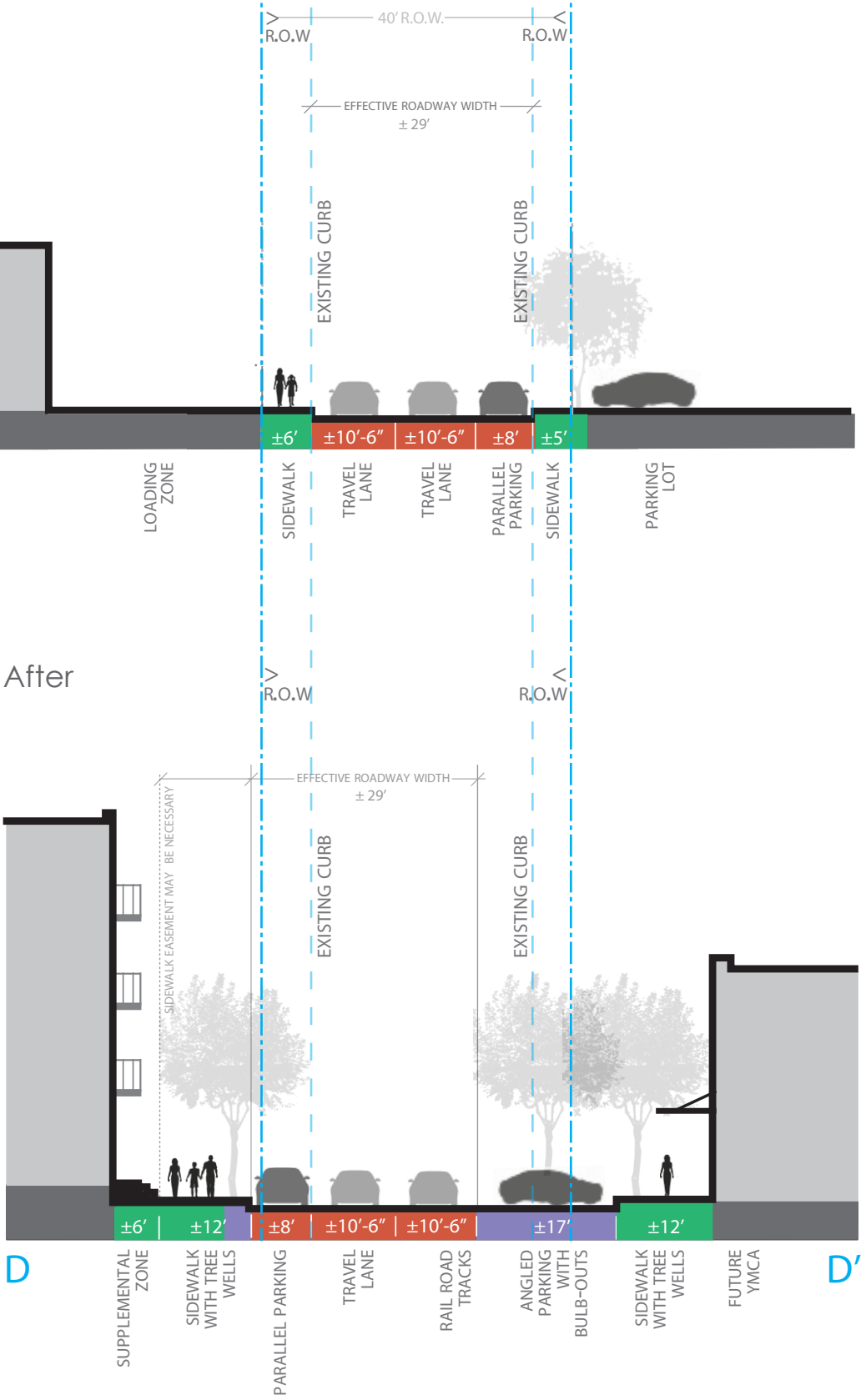
Before



See Potential Landscape Masterplan for Plan View

Intendencia Street - 1

Before



Street Section Zones

ROADWAY ZONE

BUFFER ZONE

PEDESTRIAN ZONE

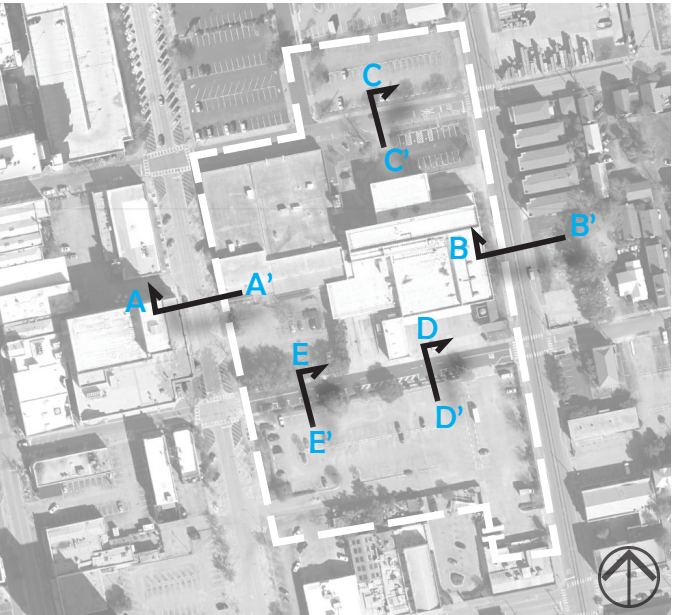
- A- Jefferson Street**

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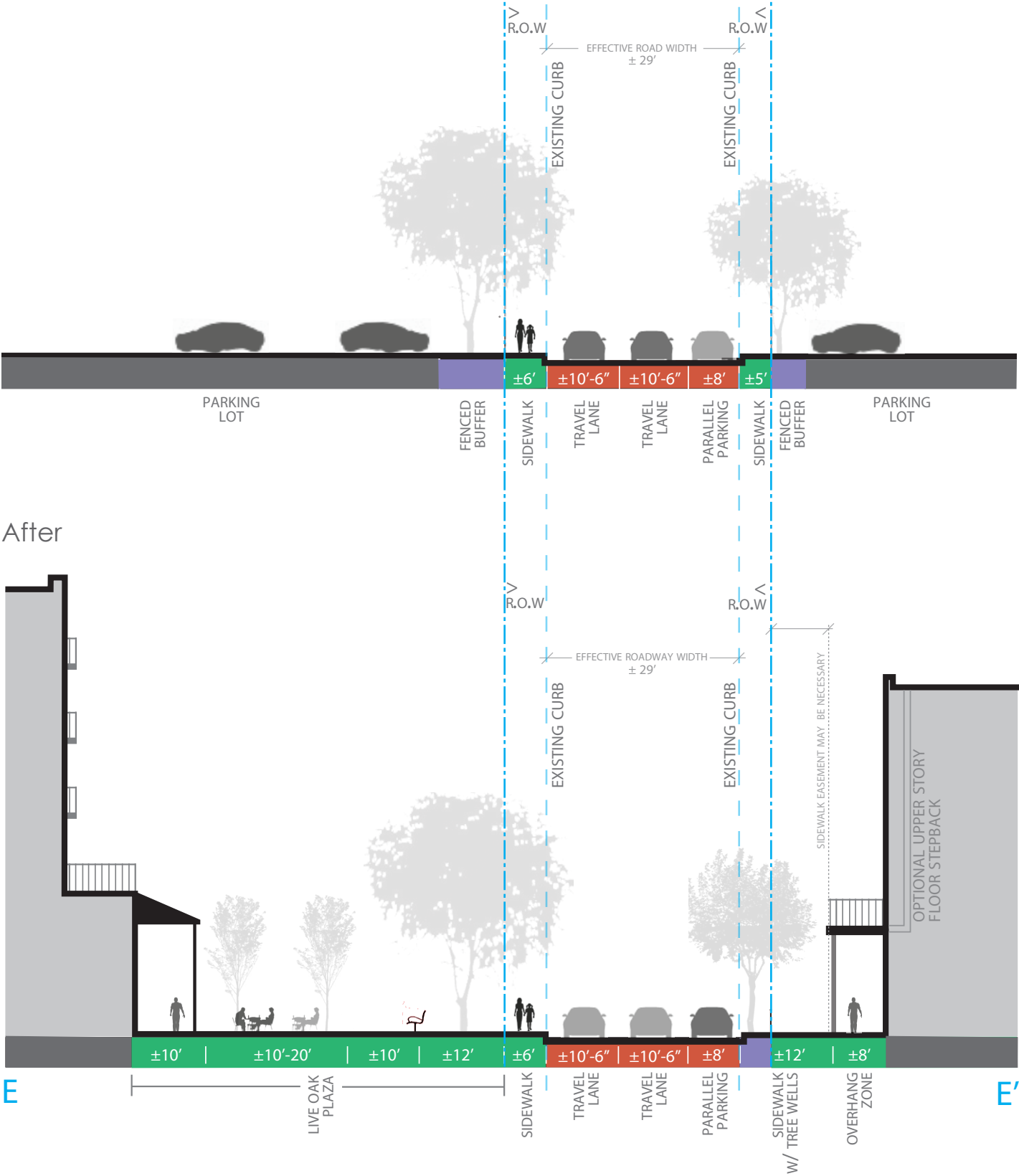
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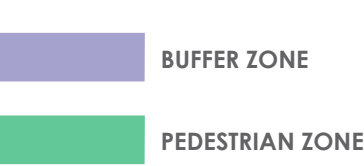
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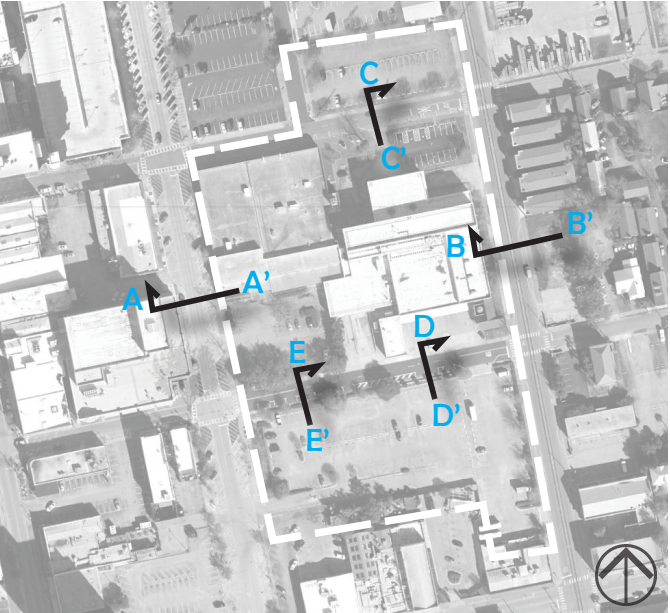
Intendencia Street - 2 Before



Street Section Zones



- A- Jefferson Street**
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See Suggested Landscape Masterplan for Plan View

PRECEDENTS



A - Urban rain gardens as part of streetscapes



B - Rain gardens as buffers



C - Greenwalls give life to otherwise bland parking deck facades



D - Private amenity spaces can use small sunken planters to delineate space and manage rainwater



E- Green Roofs



F - Rainwater harvesting cistern

Low-Impact / Green Infrastructure Types

In Florida, rainwater management largely focuses equally on water quality and quantity. All efforts should be made to spread and percolate water through natural processes in order to improve quality and reduce stress on existing infrastructure.

A- Urban Rain Gardens

Bulb-out extensions create excellent opportunities to mitigate rainwater run-off and add an aesthetic appeal to crosswalks

B- Rainwater management as a buffer

- Vegetated bioswales can be used as a tool to separate private and public.

C-Green Wall / Living Wall

- Parking deck walls can be made into aesthetic rainwater management assets

D- Sunken Planters

Planting in amenity and plaza spaces can be utilized as green infrastructure “pockets” to help localize water drainage

E- Green Roofs

Green roofs vastly decrease roof run-off and heat gain while concurrently creating an aesthetically appealing asset

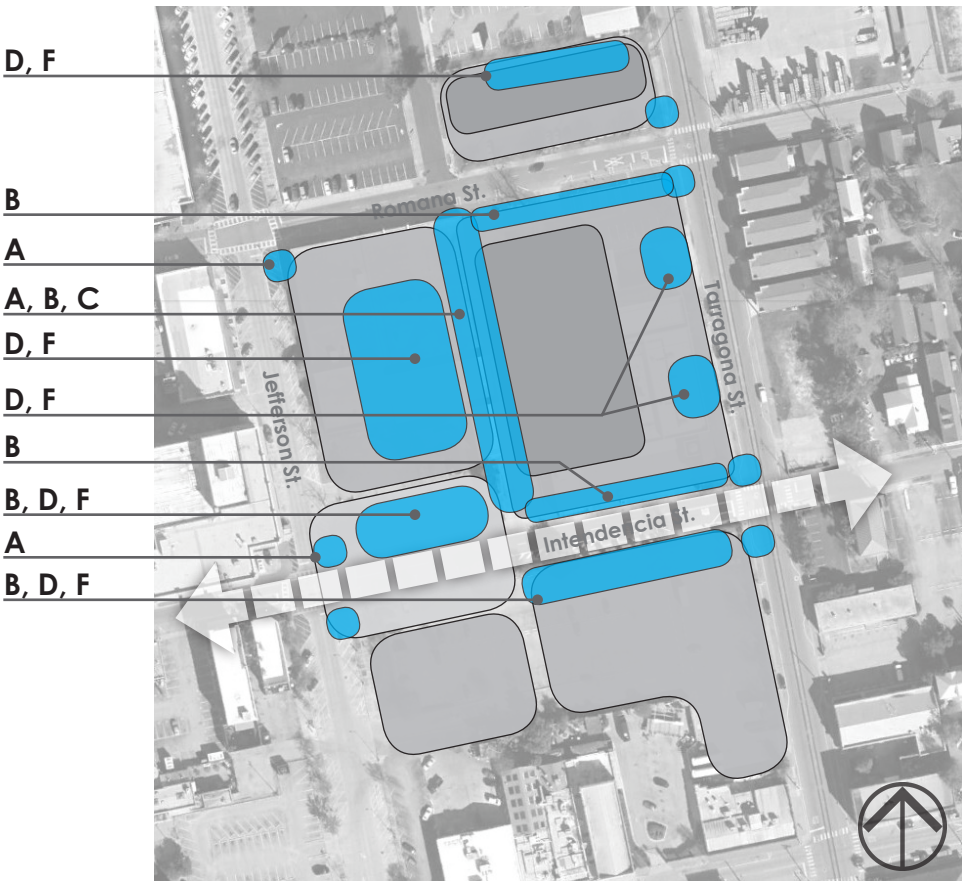
F-Rainwater Harvesting

A rainwater harvesting system reduces the amount of water flow into the existing infrastructure and can be used for irrigation of the property

Permeable Pavers

As one of the most feasible rainwater infiltration systems, permeable pavers should be installed in locations that are not likely to handle vehicles

Examples of Potential Locations





SECTION 3 | ILLUSTRATING THE FRAMEWORK

PNJ SITE PLACE-MAKING MASTER PLAN

Prepared for DAILY CONVO, LLC. by LORD AECK SARGENT

This sample site plan focuses on the development goals while utilizing the urban design framework to incorporate a context sensitive solution.

STRUCTURES

A- RESIDENTIAL MIXED USE

Residential (4 lvs on podium)	123,200 sf
x 83% efficiency	102,256 sf
/ 850 sf/unit avg amenity (podium)	120 units
loading (podium)	6,000 sf
retail (podium)	6,000 sf
	20,000 sf

B-RESIDENTIAL

Residential(4 lvs on grade)	141,240 sf
-4,000 sf amenity	137,240 sf
x 80% efficiency	109,792 sf
/ 850 sf/unit avg	129 units

C-PARKING

Parking Deck	
typical level:	28,800 sf
320 sf/ car (x 4.3 levels)	90 cars/ lvl
Total deck parking	387
On-Street parking	
Total	86
TOTAL provided	473 cars

D-YMCA (2 Levels)

	50,000 sf
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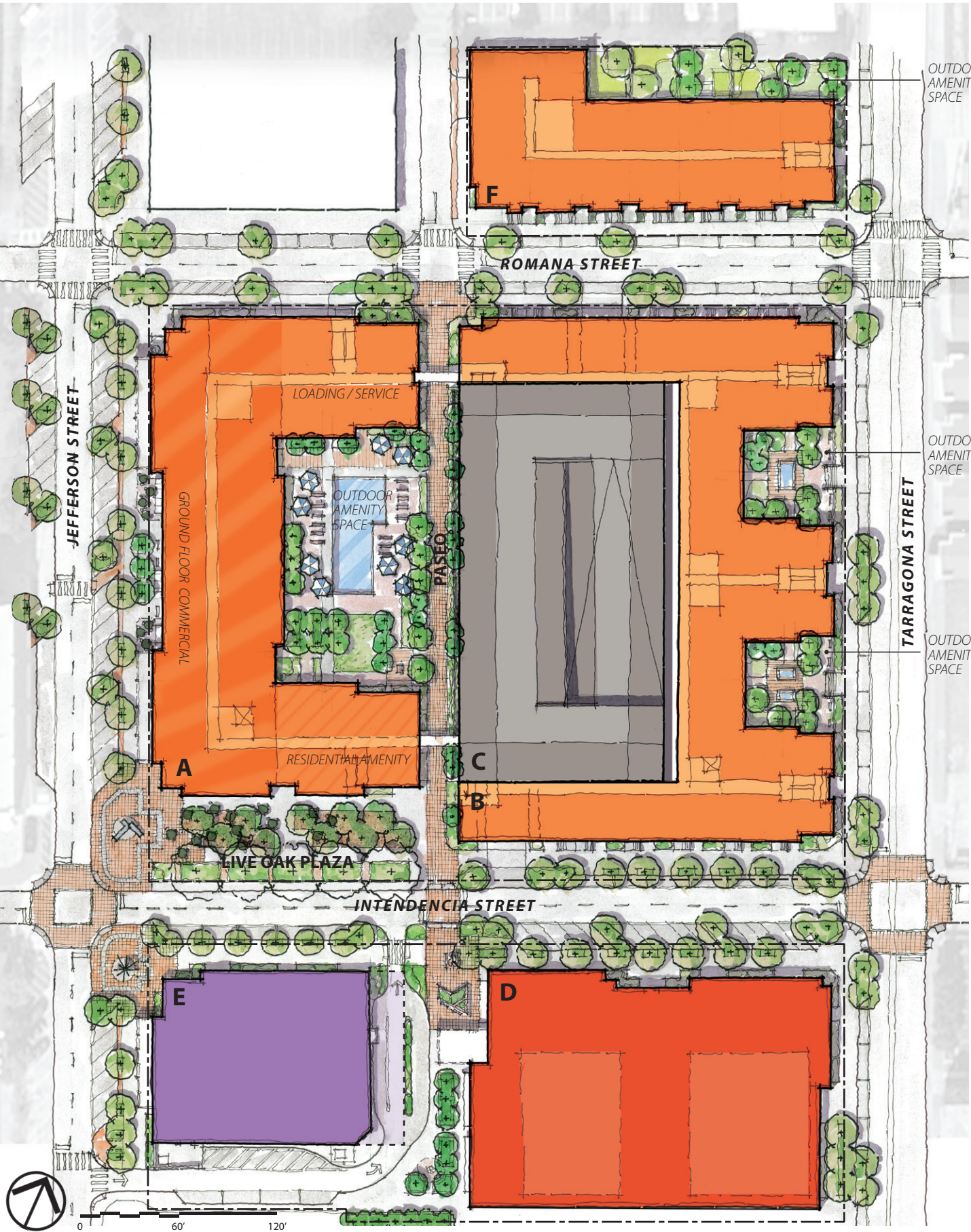
E-COMMERCIAL MIXED-USE (3 Levels)

Ground Floor Retail	13,500 sf
Office (Upper Two Levels)	32,500 sf

F-RESIDENTIAL

note: this parcel self-parks (can be phased independently)

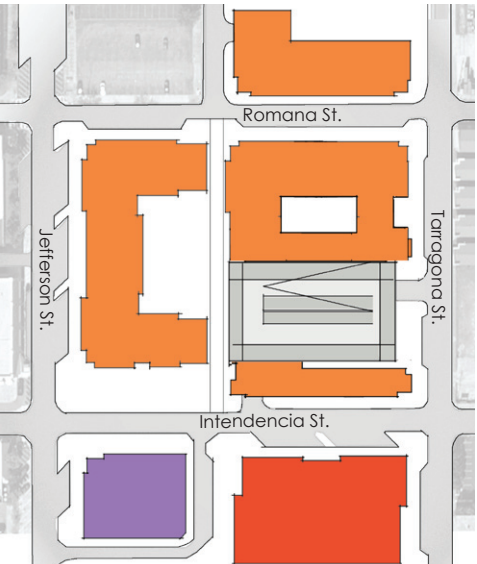
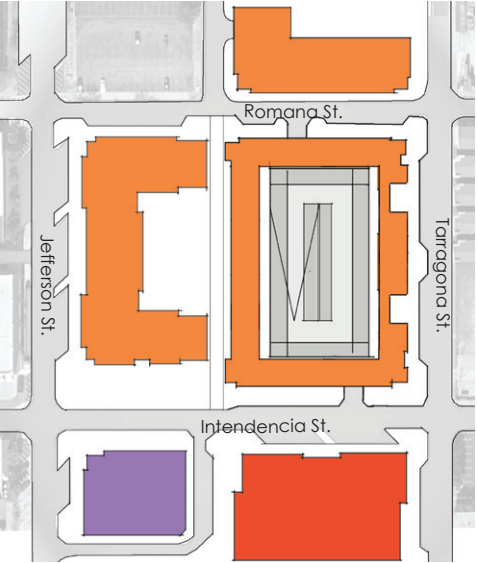
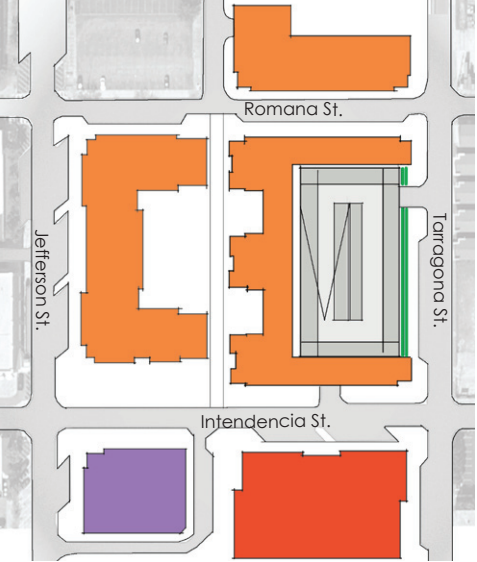
Residential	52,500 sf
-5,000 sf amenity	47,500 sf
x 83% efficiency	39,425 sf
/ 850 sf/unit avg	46 units
Parking (one level)	49 cars



- Alternative I**
- Pros**
- Internal units face paseo and amenity spaces
 - Reduced exposure to locomotive noise
- Cons**
- Distance from deck to Phase I Residential/Retail not optimal
 - Reduces the “public” atmosphere of the Paseo
 - Places “back” of development toward existing and future residential development on Tarragona
 - Scale of deck not highly compatible with adjacencies
 - Buffer required for exposed parking deck reduces the size of amenity courtyards

- Alternative II**
- Pros**
- No “back” of development
- Cons**
- Development not highly efficient
 - Increases number of residences exposed to locomotive noise
 - Distance from deck to Phase I residential/retail not optimal
 - Reduces private amenity space

- Alternative III**
- Pros**
- Further activation of “Paseo”
 - Activates Tarragona
- Cons**
- Atypical development
 - Increases number of residences exposed to locomotive noise
 - Distance from deck to Phase I residential/retail not optimal
 - Smaller private amenity space
 - Scale of deck not highly compatible with adjacencies



Pedestrian Paseo: As a publicly accessible alley, the design of this space should focus on installing unique vertical elements such as green walls, creeping vines, wall art or sculptures as a way to give character and life to the pedestrian experience.

As noted in the Open Space Types Framework, the existing historic brick pavers on the site could be repurposed here.

Jefferson Street: The street's perceived width is mainly a result of under-utilized space. A slight reduction in lane and parking widths combined with the ample existing sidewalk width allow for a human scaled "Palafox District" street.

The designed streetscape consists of a 10' walkway that follows the overhang/balcony typology found within the district and an adjacent 10' furniture zone that creates spaces for outdoor seating, street furniture and other supplementary elements. Tree islands for potential rainwater management and to reduce pedestrian crossing lengths are placed to help visually reduce the street width. (See Urban Design Framework: Proposed Streetscapes for street dimensions)

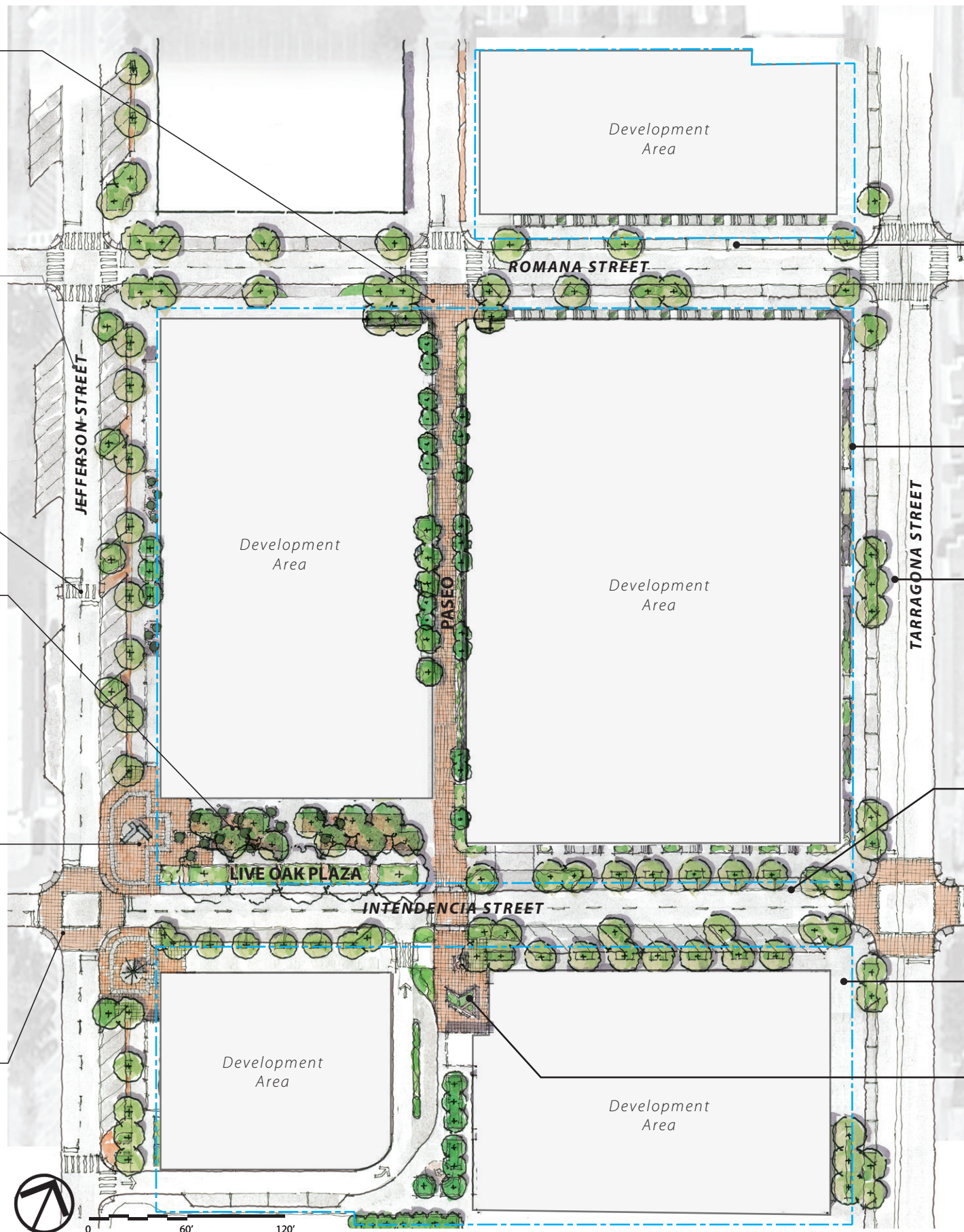
Mid-block Crossings: Given the length of the site's blocks, mid-block crossings should be utilized where possible.

Live Oak Plaza: The focal outdoor space of the development utilizes the existing row of Live Oaks and the future mixed-use building to frame a $\pm 50'$ - $60'$ wide plaza space that is flexible in use and meant to serve as a "nook" for everyday use. The central planting space can be designed as a small allee of trees in a bed of crushed brick as an area to house move-able "Paley Park" style benches and tables.

As a unifying feature, the existing historic brick pavers could be used at the Plaza entrance, in the Paseo, and the YMCA entrance Plaza. Due to their placement the plaza's existing Live Oak trees will provide an ample amount of shade through out most of the year.

Development Gateway/Wayfinding: As the focal corner of the development, a sculpture, fountain, or similar element should be installed to help convey a sense of arrival and place.

Intendencia Crosswalks: As a way to emphasize the importance of Intendencia, decorative vehicular-rated pavers should be installed



Romana Street: Designed as the secondary East-West street in the project, on-street parking with tree wells is utilized to create a more human scaled intimate streetscape. Residential entrances are treated with stoops and steps in order to contribute to that goal. Crosswalks could be treated with typical thermo-plastic striping (See Urban Design Framework: Proposed Streetscapes for street dimensions).

Potential Courtyard Entrances: In order to provide a sense of privacy and create a "wall" for an urban facade, the entrances to potential residential courtyards could be defined with planted buffers.

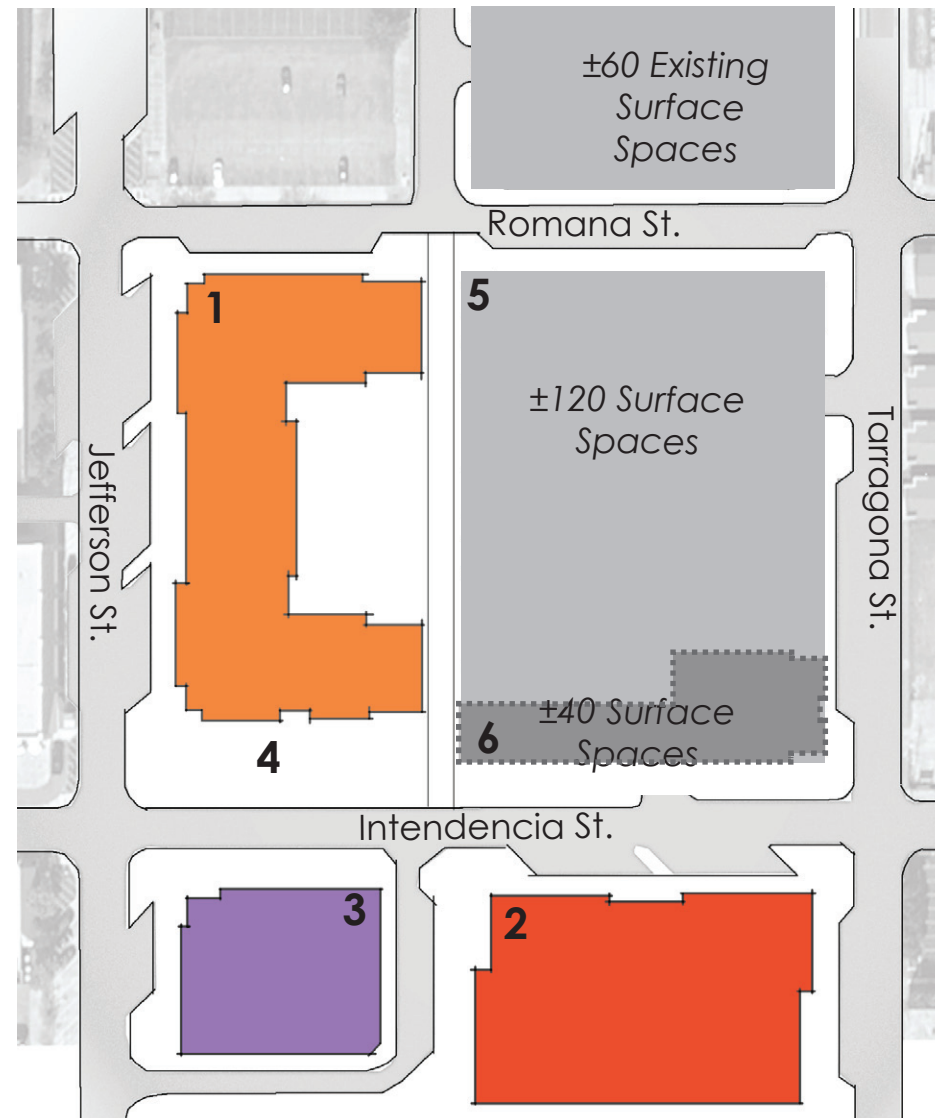
Tarragona Street: Given the emerging residential frontage on this street, it is designed to evoke the feel of a low-rise residential neighborhood in order to create a transition between Downtown and the Seville Neighborhood. Some on-street parking is removed and replaced with planted bulb-outs in order to shrink the perceived street width and provide morning shade. (See Urban Design Framework: Proposed Streetscapes for street dimensions).

Intendencia Street: As the focal street of this project, Intendencia is treated with larger sidewalks and trees while keeping its narrow width to maintain the existing human scale. In addition to the existing parking, some angled spaces were added in order to accommodate high turn-over parking needed for the YMCA. (See Urban Design Framework: Proposed Streetscapes for street dimensions).

YMCA/Development Wayfinding: In order to increase presence along Tarragona Street, implement a wayfinding or gateway element here for the YMCA + development.

YMCA Entrance: In order to further embrace the sense of place created by the Live Oak Plaza and the Paseo, the YMCA entrance is proposed as a small plaza at the confluence of the two elements. Remaining historic brick pavers not utilized in the Paseo or Intendencia corners could be installed here to help further unify this space.

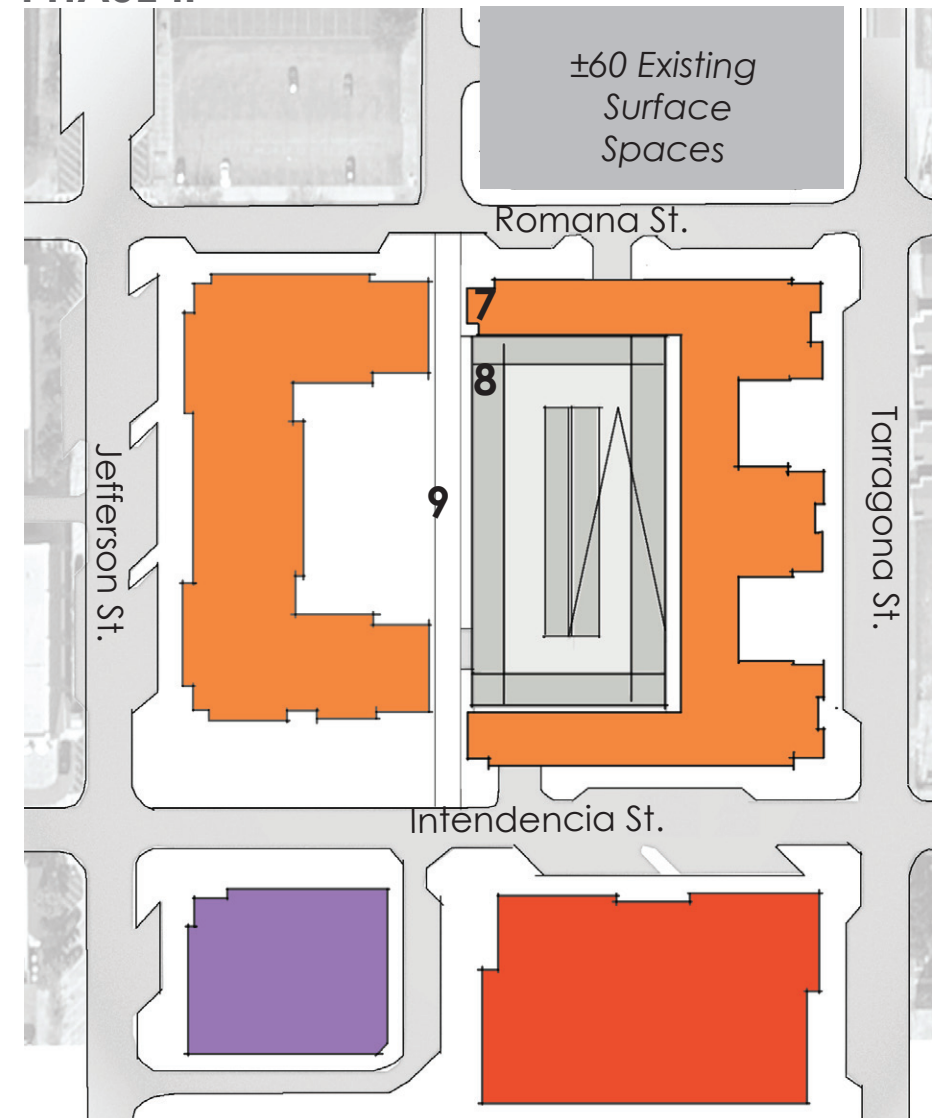
PHASE I



Phase I Elements

- 1: Mixed-Use Residential Building (5 levels)
 - 2: YMCA Building (2 levels)
 - 3: Mixed-Use Commercial Building (3-4 levels)
 - 4: Live Oak Plaza
 - 5: Surface Parking Lot(s)
 - ±220 off-street parking spaces
 - ±85 on-street parking spaces
 - 6: Potential Residential Wing- Reduces capacity of surface lot parking/ increases short-term parking requirement for Phase I
- Streetscape Improvements*

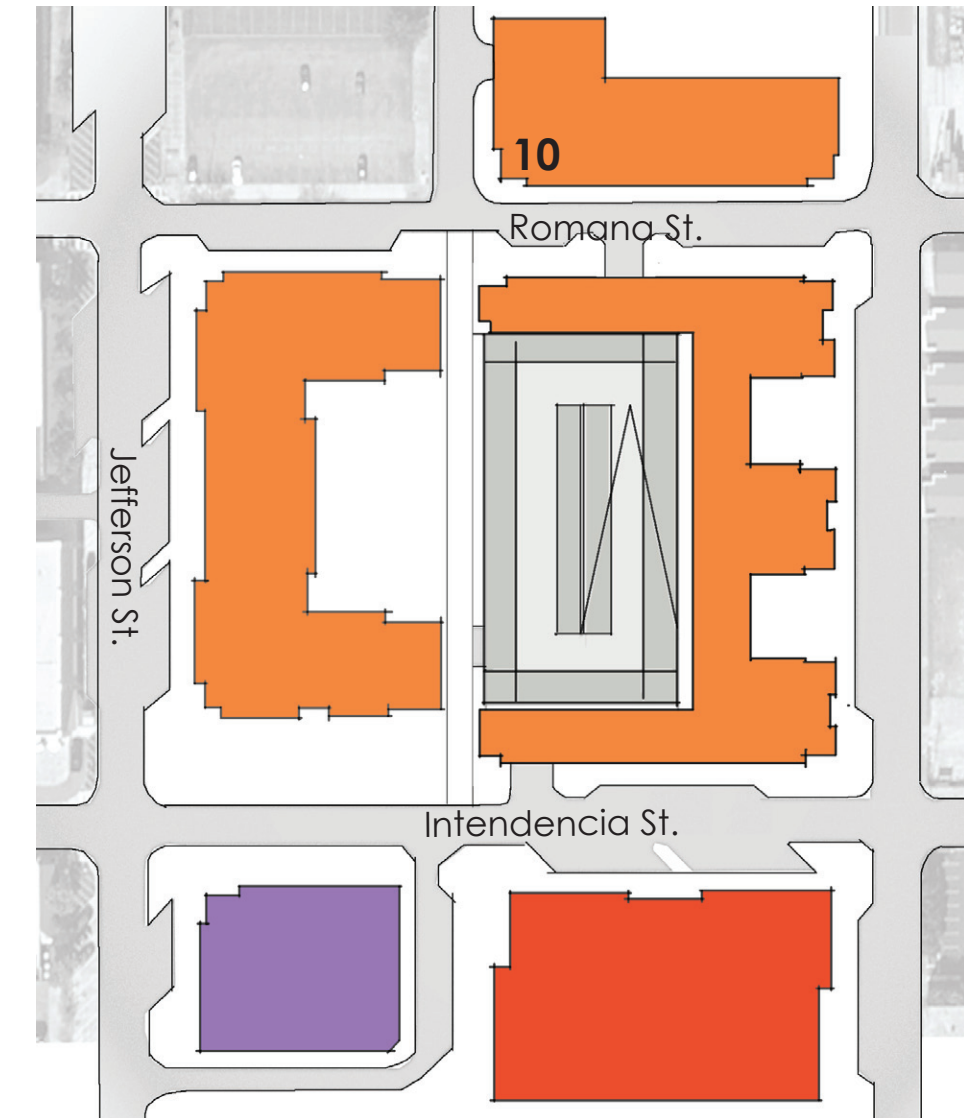
PHASE II



Phase II Elements

- 7: Residential Building (4 levels)
 - 8: Parking Deck (±4.5 levels)
 - 9: Paseo
- Streetscape Improvements*

PHASE III



Phase III Elements

- 10: Residential Building (±3 levels)
- Streetscape Improvements*

